

WATERFRONT DISTRICT MASTER PLAN
CITY OF HERCULES, CALIFORNIA



THE BIXBY COMPANY
ADOPTED BY CITY COUNCIL 25 JULY 2000
AMENDED SEPTEMBER 2001

SARGENT TOWN PLANNING

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This document has been prepared for, and in collaboration with, The Bixby Company, LLC. Sargent Town Planning, LLC. is the master planner and principal author of this document. The architectural design and 3-dimensional computer modeling of the buildings, and most of the Architectural Regulations, have been prepared by John Reagan Architects. The Landscape Regulations have been prepared by CPS Landscape Architects. This is a working draft of the document, subject to further revisions and expansions as further phases of District development are documented. Sargent Town Planning may be contacted at 805-644-1892 or at townplan@pacbell.net.

1.0 INTRODUCTION

1.1 LOCATION AND SETTING

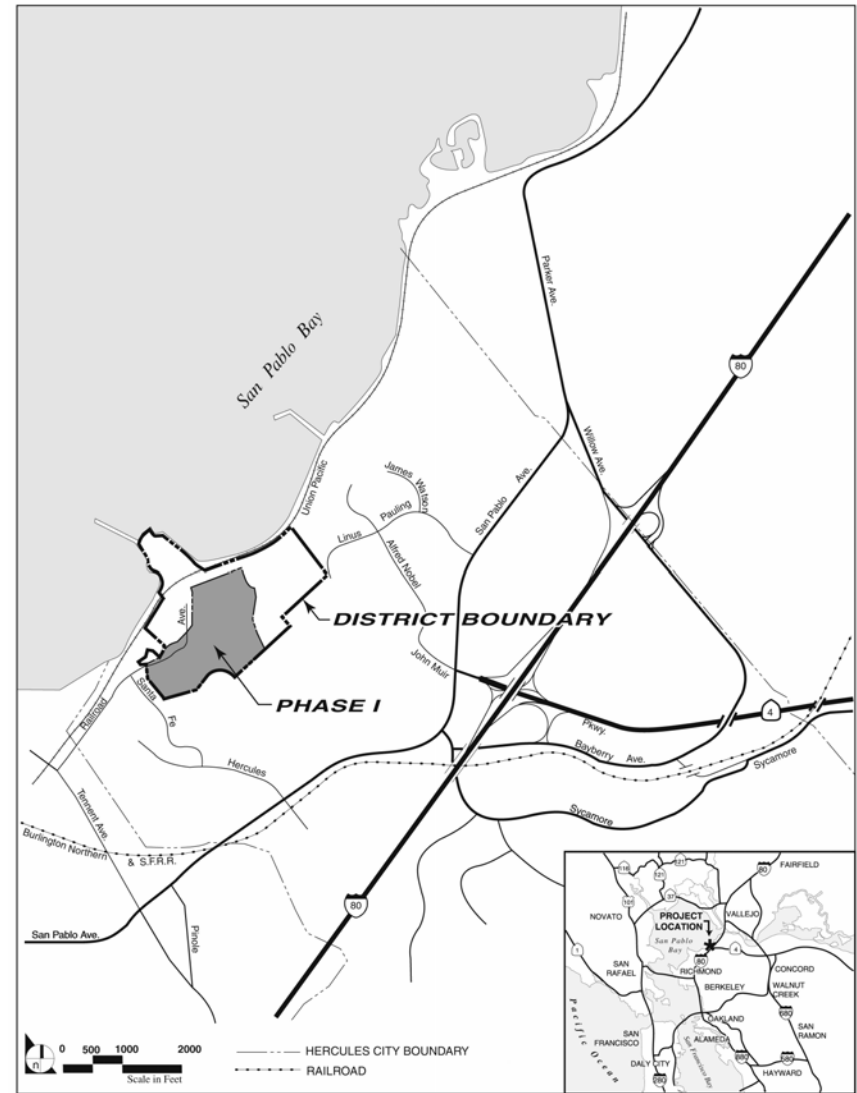
The Hercules Waterfront District (“the District”) is located within the former Hercules Powder Company property in the City of Hercules, west of I-80 (see map below). The entire property encompasses about 167 acres, and is generally bounded by San Pablo Bay and the Union Pacific Railroad to the north; Talley Way, existing residential development and State-owned land to the west; the Remediation Financial, Incorporated (RFI, formerly Geysler) site, Hercules Wastewater Treatment Plant, existing residential development, and the Citation residential project to the south; and the North Shore business park to the east.

General Plan Land Use Designations within the District (as shown on the General Plan Land Use Map) include Waterfront Commercial; General Commercial; Historic Town Center; Industrial Residential; Multi-Family Low Density; Multi-Family Medium Density; Planned Office/Research & Development; and Public/Open Space. The General Plan also applies Special Study Area designations to District land: Special Study Area No. 1, Mixed Use Planned Development, and Special Study Area No. 2, Residential Planned Development.

A Refined General Plan concept approved by the City in July 1999 applies a range of options to portions of the property, including Planned Commercial Residential; Historic Town Center; Planned Office/R&D; Multi-Family Medium Density; and Multi-Family High Density.

Zoning designations within the District (as shown on the City Zoning Map) include: WC, Waterfront Commercial; GC, General Commercial; HTC, Historic Town Center; IR, Industrial Residential; RM-L, Residential Multiple Family Low-Density; RM-M, Residential Multiple Family Medium-Density; PO-RD, Planned Office-Research and Development; P/QP-OS, Public/Quasi-Public Open Space; and S-P-O, School Park Overlay.

The District property is generally known as the Hercules Properties Inc. (HPI) site, and is so designated on the General Plan Land Use Map. The Bixby Company, LLC., acting as master developer for the property, has cooperated with the City of Hercules to prepare this Waterfront District Master Plan to guide the development of the property, and ensure that the goals and objectives of the General Plan are realized.



District Location and Vicinity Maps



CONCEPTUAL DISTRICT MASTER PLAN

1.0 INTRODUCTION

1.2 PURPOSE AND INTENT

The General Plan and Zoning Ordinance require that land within the HPI Property be developed as a fully master planned and integrated community. Accordingly, this District has been created to accomplish the following specific purposes, as set forth in those documents:

1. Recognize and preserve the historic character of the existing Hercules Powder Company town site, and allow reuse of existing structures, where appropriate;
2. Allow the construction of new buildings while maintaining the architectural quality of the District;
3. Provide a network of public spaces that have access to views of Hercules Point and San Pablo Bay;
4. Provide access along the Bay shoreline that does not degrade its existing natural resources;
5. Provide a location for a commuter rail station with adjacent mixed-use development;
6. Provide for a variety of professional, administrative and personal service offices, as well as retail businesses that support the offices and provide services and goods to visitors to the waterfront area;
7. Provide the opportunity to accommodate both residential and commercial uses in a well planned, mixed-use development;
8. Provide public spaces and facilities that offer a balance of open space, landscaping,

recreation space, transit access, and shared parking facilities.

9. Allow lower cost live-work opportunities for start-up businesses that are compatible with the residential and commercial uses within the District;
10. Provide the opportunity for upper floor residential uses over ground floor commercial uses;
11. Provide for careful design review of proposed buildings to ensure that the benefits of mixed-use development are fully realized and the potential negative impacts of one use upon another are minimized.

1.3 STRUCTURE AND AUTHORITY

The Waterfront District Master Plan includes the following major elements to guide the design and design review of development within the District:

The Public Space Master Plan: This defines the network of circulation and open space elements that are the framework of public space that organizes and connects the community. The public space network is substantially fixed in location, configuration and design by the Plan. This network divides the private land into blocks. The Public Space Master Plan consists of two elements:

1. The Circulation Master Plan: This fixes the location and design of all public ways within the District.
2. The Open Space Master Plan: This fixes the location and design of parks, greens, plazas,

and trails within the District.

The Code: This defines the manner in which lots within the blocks may be developed. The Code includes the following:

1. The Regulating Plan: This plan indicates which Types of buildings may be built within a block or on a lot.
2. Urban Regulations: These define the permitted Building Types. For each Type, specific development and use standards are defined, regulating the location of the building (s) on each lot, the permitted configurations of the buildings, the required locations and sizes of yards, and the permitted locations of various uses within the building(s) and on the lot.
3. Architectural Regulations: These define the permitted architectural styles, elements, materials, techniques, and details. The intent of these regulations is to limit the range of architectural expression within the District to ensure a reasonable level of architectural harmony, so that the public spaces defined by the private development will be well-defined outdoor spaces, conducive to the active public life envisioned by the General Plan.
4. Landscape Regulations: These define the required and permitted landscape and hardscape elements, both within public spaces and on private lots. The urban design of the public realm – the streetscape of the District thoroughfares, the hardscape of public plazas, and the landscape of parks, greens and

1.0 INTRODUCTION

other open spaces – defines the space and character of the District. The landscape and hardscape of the frontages and yards on private lots supports the streetscape design and provides high-quality outdoor recreation opportunities on the lots.

AUTHORITY

As required by Chapter 48 of the Zoning Ordinance, this Master Plan has been prepared to meet the requirement for a Planned Development Plan. Upon final adoption of this Master Plan by the City Council, individual projects proposed within the District that are found by the Community Development Director (the “Director”) to be in substantial conformance with the requirements of this Master Plan may be permitted by Community Development Director Review and Action, in accordance with Chapter 42 of the Zoning Ordinance.

This Master Plan (“Plan”) is being adopted by the City Council as an Ordinance to regulate development within the District. In matters on which this Plan is silent, the provisions of the Zoning Ordinance shall apply. In the case of any conflict between provisions of the Zoning Code and the provisions of this Plan, the provisions of this Plan shall take precedence. In the case of a conflict between this Plan and the Uniform Building Code, the Building Code shall take precedence. In the design of designated historic structures within the District, the State of California Historic Building Code may take precedence over the Uniform Building Code.

1.4 MASTER PLAN CONCEPT

To implement the mixed-use district concept called for by the General Plan and Zoning Ordinance, the Waterfront District Master Plan organizes development within its boundaries as a traditional neighborhood and several distinct mixed-use sub-districts. These are shown on the Conceptual District Master Plan on the following page. These six planning sub areas will be developed in phases.

The circulation master plan has been designed to fully interconnect all six areas, and conceptual planning and urban design has been completed for areas 1 and 2. This document describes in detail the development intent and regulations for Area 1, the Waterfront Neighborhood, only.

The total gross area land within the District is approximately 163 acres. The approximate areas of the planning sub areas are listed below. This listing is in the order in which the phases are currently envisioned to be entitled and constructed. The phasing plan may be amended in the future with the approval of the Director.

1. Waterfront Neighborhood 47 acres
2. Historic Town Center 17 acres
3. Refugio Creek Nghd. 10 acres
4. Transit Village 22 acres
5. Hercules Pt. & Bay Parcels 67 acres

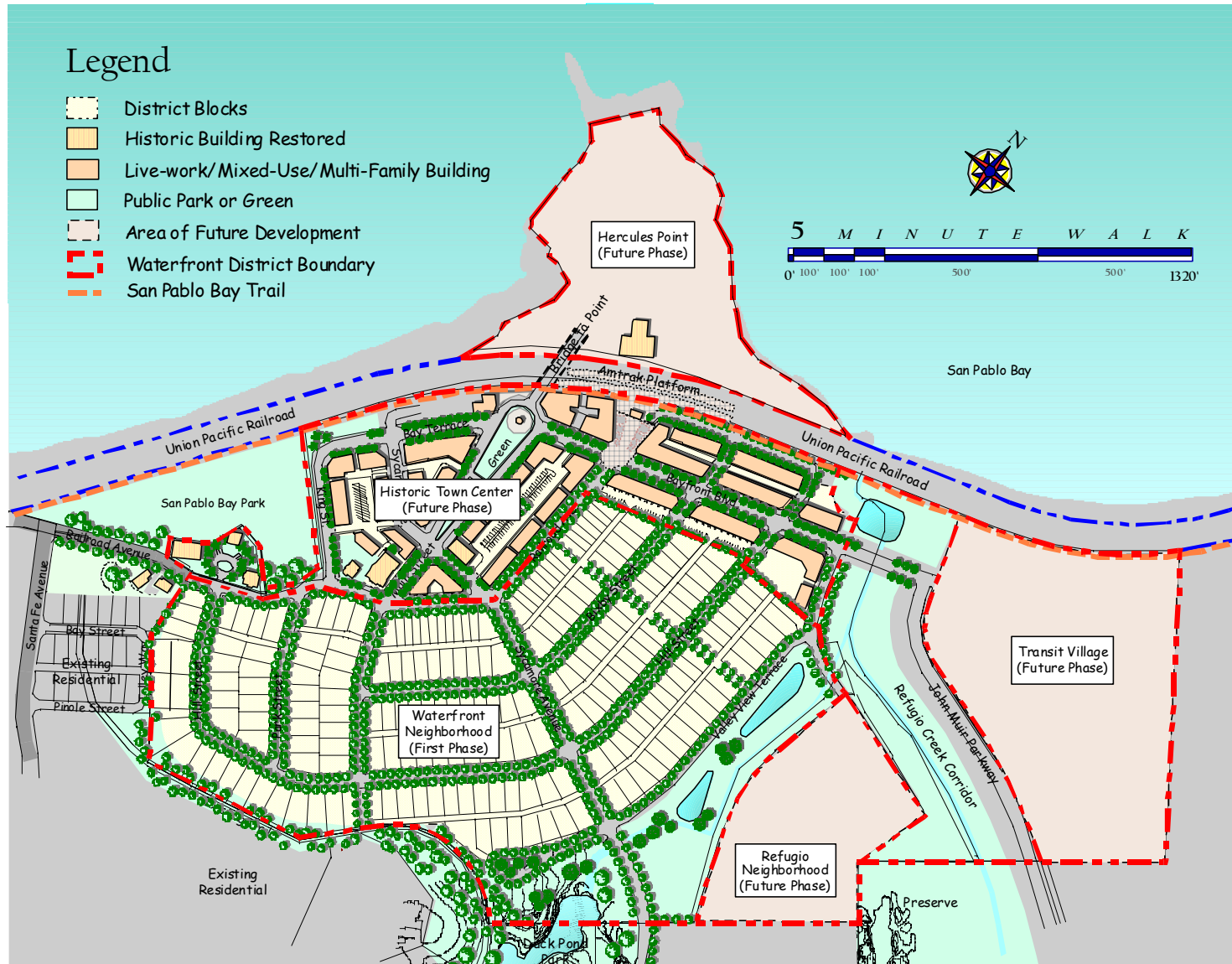
1.5 NEIGHBORHOOD BUILD-OUT

The Neighborhood planning area of the Master Plan is organized into blocks by the circulation master plan. The preliminary lot sizes and configurations have been assigned to the blocks. The number and sizes of the lots are consistent with the environmental documentation of the Master Plan, and with the Building Types permitted on the lots by the Code.

The final build-out of the Neighborhood may result in a lower lot count than shown here, and in a different distribution of lot sizes. The minimum permitted lot size is 4,400 s.f. No maximum lot size is prescribed, but 5,500 s.f. has been used for the balance of the lots, both alley-loaded and street-loaded lots. Acreages

Blk.	Area (Acres)	Units	D.U.A. (net)	Lot Sizes
1	3.8636	19	4.92	5500 s.f.
2	2.2348	15	6.71	5500 s.f.
3	2.8116	20	7.11	5500 s.f.
4	2.0883	17	8.14	4400 & 5500 s.f.
5	3.0472	23	7.55	4400 & 5500 s.f.
6	3.175	21	6.61	4400 & 5500 s.f.
7	4.6885	38	8.10	4400 & 5500 s.f.
8	3.6875	32	8.68	4400 s.f.
9	2.6572	22	8.28	4400 s.f.
Total	28.25	207	7.33	<i>Plus 33- 2nd Units</i>

listed are net of streets.



PLANNING SUB-DISTRICTS

2.0 PUBLIC SPACE MASTER PLAN

2.1 CIRCULATION MASTER PLAN

The framework of the District Master Plan is the street system. Key considerations in the planning of the District circulation pattern – arising from the City’s General Plan and Zoning Ordinance – include:

- Preserve the historic character of the Historic Town Center;
- Promote the development of Railroad Avenue into the “Main Street” of the District;
- Incorporate visual and physical access to the Bay shoreline into project designs;
- Establish view corridors and viewpoints to protect and promote views to the Bay;
- Promote safe and well-designed neighborhoods and encourage new residential development to have an internal circulation system including pedestrian walkways, bikeways, and access to transit facilities.
- Residential streets should be designed in relation to needed capacity and the adjoining housing patterns, while discouraging through traffic on local streets.

In response to these directives, the circulation system for the District has been developed as a fully inter-connected network of pedestrian-oriented thoroughfares, as delineated on the following pages. Safe and pleasant pedestrian ways are incorporated into all thoroughfare types, appropriate in width and detailing to the

planned land uses of the abutting frontages. The street widths have be minimized to promote safe and reasonable vehicular driving speeds, to facilitate safe crossing points for pedestrians by minimizing required crossing times, and to strengthen the historic town character of the District.

On-street parking is provided on all streets to encourage motorists to become pedestrians within the District. The amount and configuration of parking on each street type is gauged to support the adjacent land uses.

Landscape and hardscape are integral components of the thoroughfare types, and are designed to reinforce the spatial characteristics that create the District’s historic town character and make the District attractive to pedestrians. These elements are defined in general on the following pages, and in detail in the Landscape Regulations.


2.1.1 GENERAL REQUIREMENTS

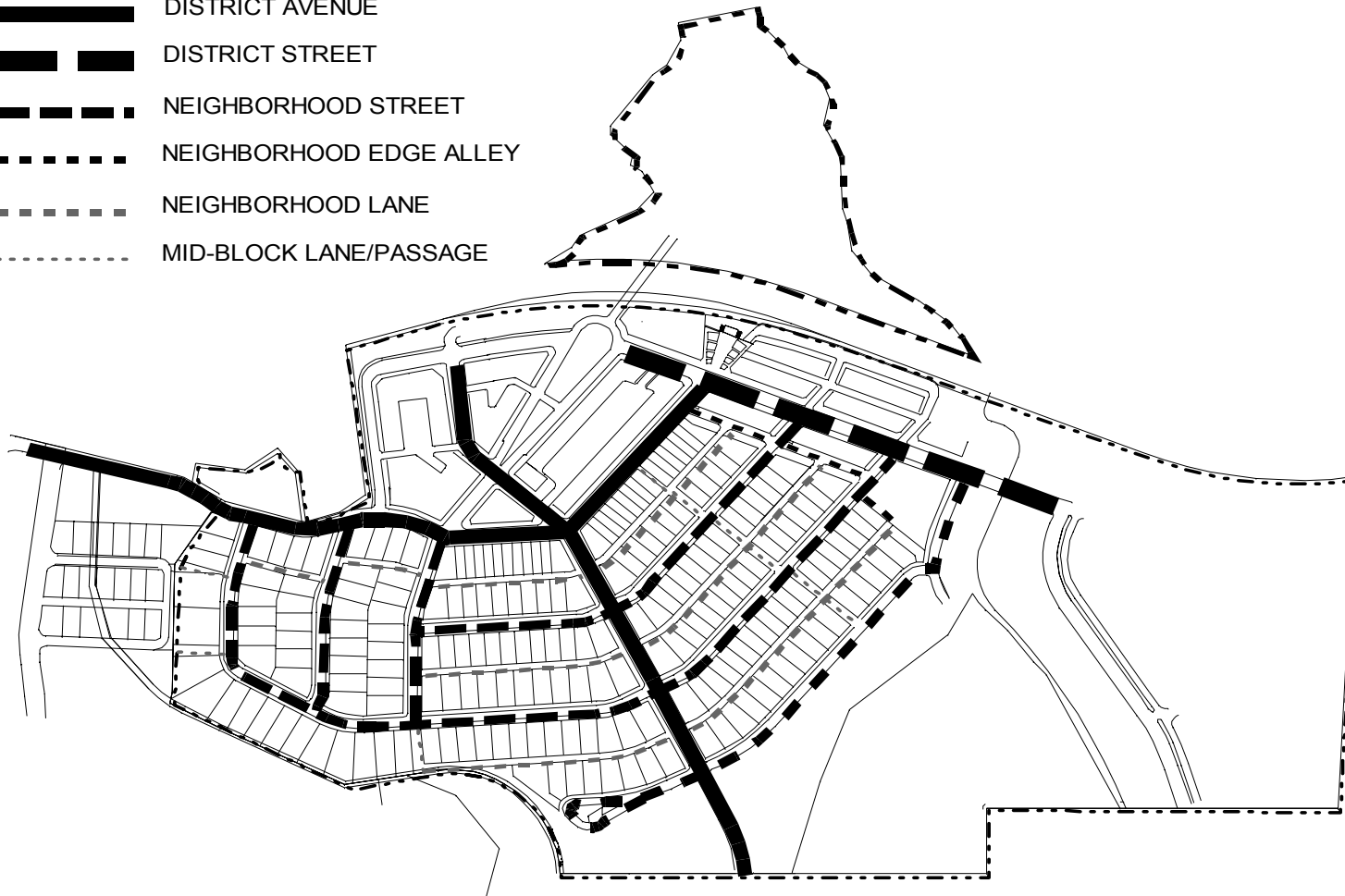
Unless noted otherwise, the following design characteristics shall apply to all thoroughfares within the District.

- Pavement of vehicular ways shall be asphaltic concrete.
- Curbs, gutters and sidewalks shall be natural concrete at a minimum. Enhanced sidewalk materials and finishes shall be provided where required by other sections of this Master Plan. All enhanced pavement shall be approved by the Director.

- Street trees shall be as specified in this section and in the Landscape Regulations. Trees shall be spaced between 25 and 35 feet on center.
- Street trees shall generally be planted in parkway strips between the curb and sidewalk along residential frontages. The trees shall be maintained by the City, and other plantings within the parkway strip shall be irrigated and maintained by the owner of the abutting property.
- On-street curbside parking shall be permitted along both sides of all streets, except where specifically noted otherwise. Parking will be prohibited within approximately 25 feet of intersections to provide a reasonable effective turn radius for larger vehicles.
- Lanes and alleys shall be public access easements on private property. Parking shall be prohibited within the access easement.
- A Maintenance Improvement District shall be formed by separate ordinance to maintain specified improvements, including but not limited to street trees, alley pavement and utilities, and street lighting.
- Property owners shall maintain all other alley improvements, including but not limited to landscaping, lighting and fencing. Owners shall also irrigate and maintain parkway strip plantings other than street trees.

2.0 PUBLIC SPACE MASTER PLAN

-  DISTRICT AVENUE
-  DISTRICT STREET
-  NEIGHBORHOOD STREET
-  NEIGHBORHOOD EDGE ALLEY
-  NEIGHBORHOOD LANE
-  MID-BLOCK LANE/PASSAGE



CIRCULATION MASTER PLAN

2.1.2.1 SYCAMORE AVENUE – SOUTH OF DUCK POND PARK

Sycamore Avenue is the primary north-south thoroughfares in the City of Hercules. See the District Vicinity Map on page 1. It crosses under the I-80 Freeway at an interchange, and connects north and south through the center of the City.

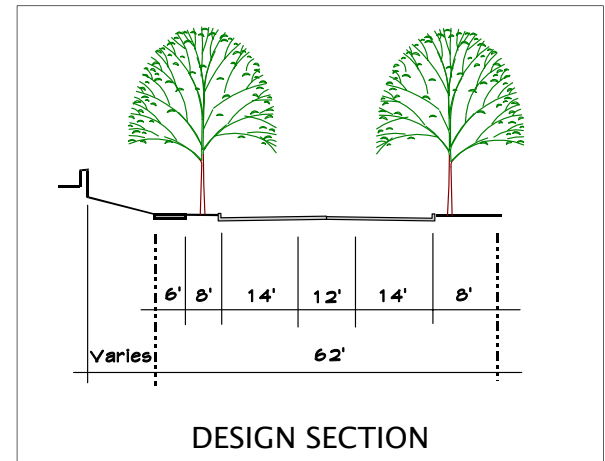
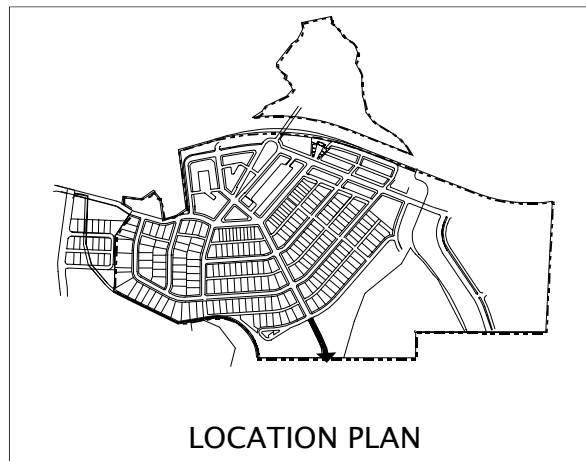
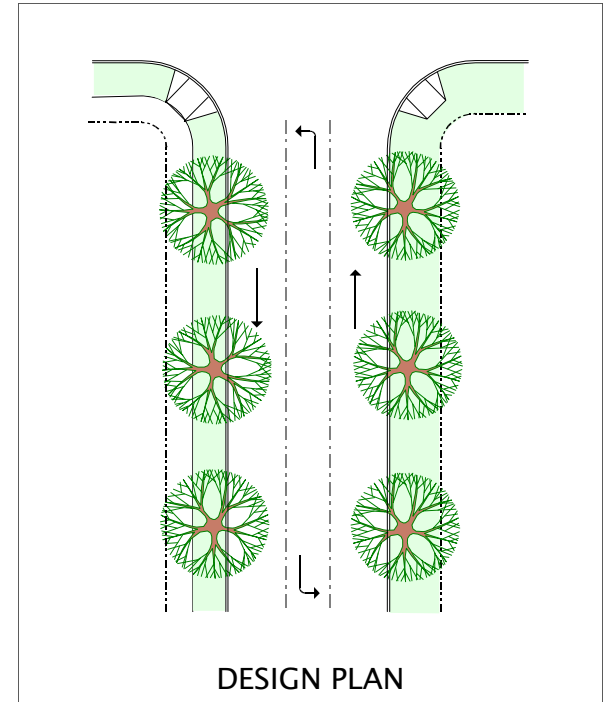
To the south of the Freeway, Sycamore passes through the City’s primary retail district and passes the Civic Center complex on the way to the City’s northerly neighborhoods.

To the north of the Freeway, Sycamore Avenue intersects San Pablo Avenue. Sycamore is planned to extend north from San Pablo Avenue to San Pablo Bay. It will terminate into Bay Terrace in the Waterfront District.

The design of Sycamore Avenue within this portion of the District is that of a road passing through a natural open space. The Avenue is flanked on one or both sides with open spaces, including the parks along the east edge of the Neighborhood, and the natural open space of Refugio Creek and its tributaries.

DESIGN NOTES:

- ♦ Curbside parking is prohibited on both sides of this street.
- ♦ The pavement width will accommodate left turn pockets where required by the City engineer. Pockets, if provided, shall be defined with pavement striping.
- ♦ A sidewalk is provided only on the west side of the street, abutting the parks.
- ♦ District standard streetlights shall be provided along both sides of the street, spaced at approximately 60 feet on center. See Landscape Regulations for detailed information about streetlights and other urban design accessories.



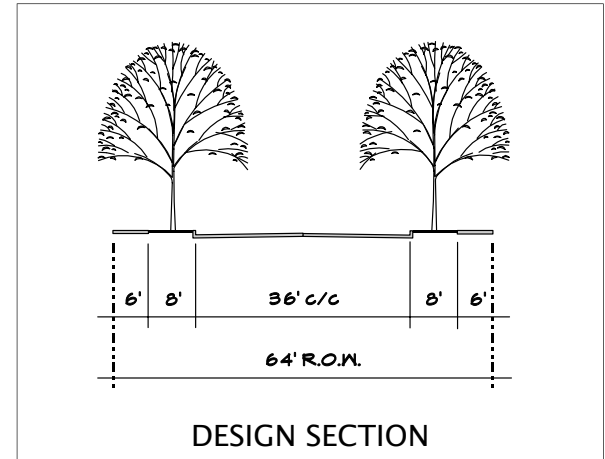
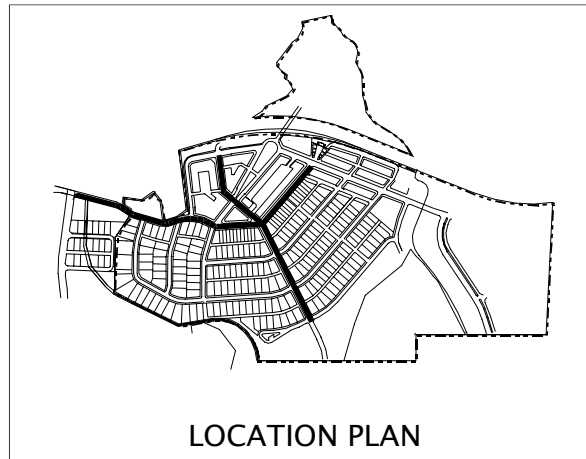
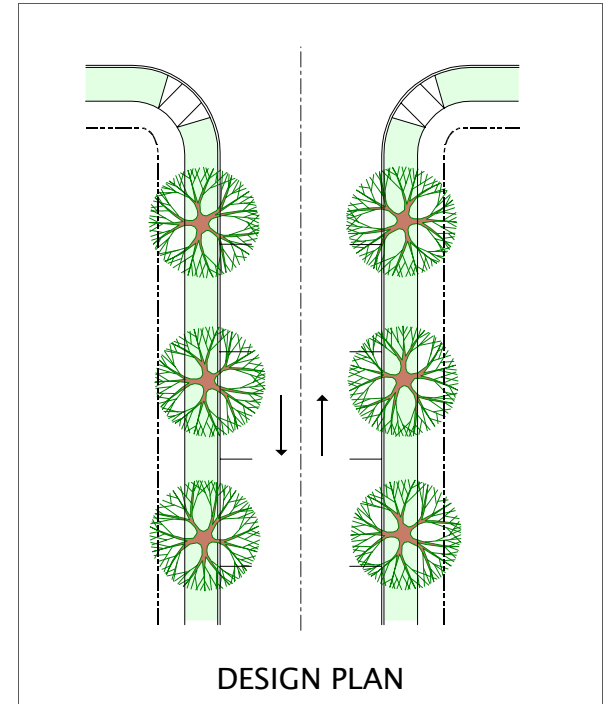
2.1.2.4 RAILROAD AND SYCAMORE AVENUES

These Avenues provide access into and through the Neighborhood. It has been located and designed with the following specific intentions:

- Provide convenient and safe vehicular and pedestrian access through the Neighborhood and to and from adjoining neighborhoods and districts;
- Accommodate higher levels of traffic than the typical neighborhood streets. By their location and design, the Avenues will carry most of the through traffic, allowing the narrower neighborhood streets to remain quiet and pleasant for children and other pedestrians.
- Provide high quality public spaces that the residences can attach to with front doors, generous windows, front porches and semi-public front yards. This type of street supports the value of the traditional house types planned for the Neighborhood, and encourages social use of front yards and porches.
- Provide convenient parking for visitors at the curbside. When frontages are developed with the alley-loaded building types recommended by the Code, the entire length of the curbs is available for this purpose. The rows of parked cars, along with the street tree and parkway strip plantings, provide a strong separation of the pedestrian ways from the traveled way.

DESIGN NOTES:

- Curbside parking is permitted on both sides of these streets.
- Street trees are planted in continuous parkway strips on most frontages. Along the westerly side of Railroad Avenue abutting the Historic Town Center, the parkway strip may be paved with unit pavers in between the trees if approved by the Director.
- District standard streetlights shall be provided along both sides of the street, spaced at approximately 60 feet on center. See Landscape Regulations for detailed information about streetlights and other urban design accessories.
- The names of the avenues may be changed at the time of right-of-way dedication, with the approval of the Director.



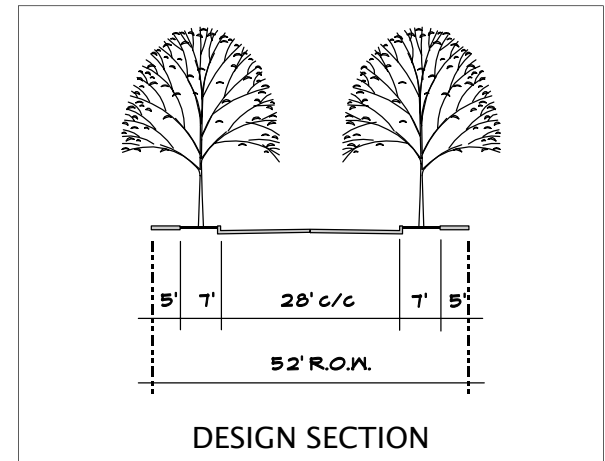
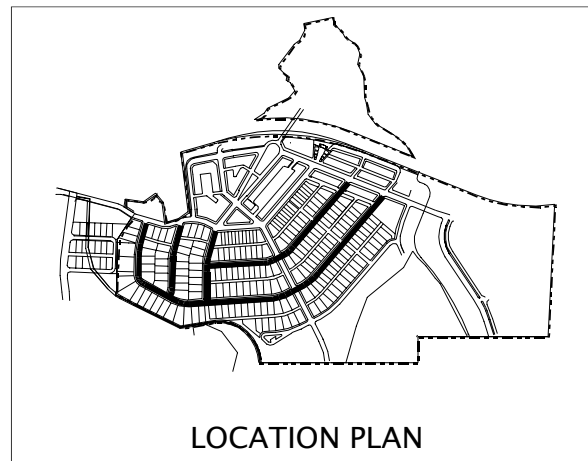
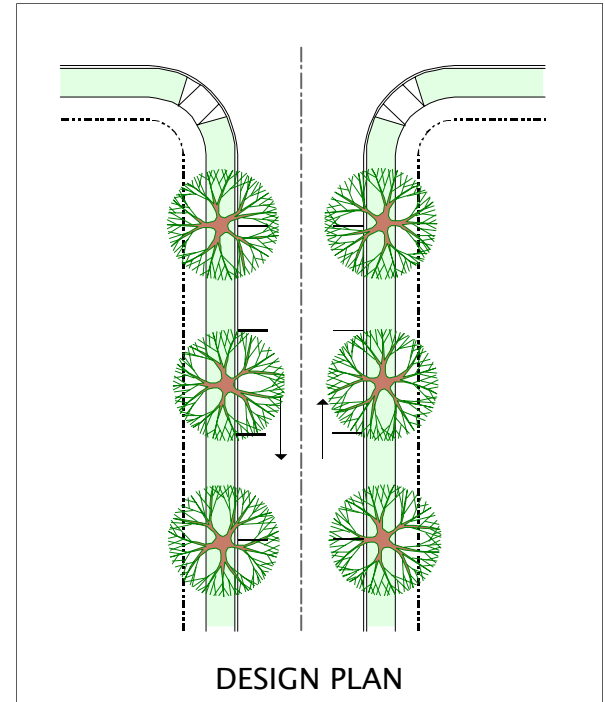
2.1.2.5 NEIGHBORHOOD STREETS

Neighborhood Streets are the heart of the public space within the Neighborhood. They are designed with the following specific intentions:

- ♦ Provide convenient and safe vehicular and pedestrian access within the neighborhood, and to and from adjoining neighborhoods and districts;
- ♦ Provide high quality public spaces that the residences can attach to with front doors, generous windows, front porches and semi-public front yards. This type of street supports the value of the traditional house types planned for the Neighborhood, and encourages social use of front yards and porches.
- ♦ Provide convenient parking for visitors at the curbside. When frontages are developed with the alley-loaded building types recommended by the Code, the entire length of the curbs is available for this purpose. The rows of parked cars, along with the street tree and parkway strip plantings, provide a strong separation of the pedestrian ways from the traveled way.
- ♦ Accommodate a range of vehicle sizes and types in a relatively narrow pavement, using the principles of a “queuing street”. This method supports low vehicle speeds in cases where traffic volumes are very low. The wider Avenues the provide access to and through the Neighborhood will be the more heavily traveled routes.

DESIGN NOTES:

- ♦ Parking is permitted on both sides of these streets.
- ♦ Parking is prohibited within 25 feet of corners to allow safe turning movements by occasional large vehicles.
- ♦ The street trees shall be maintained by the City. All other landscaping within the parkway strips shall be irrigated and maintained by owner of the adjoining property.
- ♦ District standard streetlights shall be provided along both sides of the street, spaced at approximately 60 feet on center. See Landscape Regulations for detailed information about streetlights and other urban design accessories.



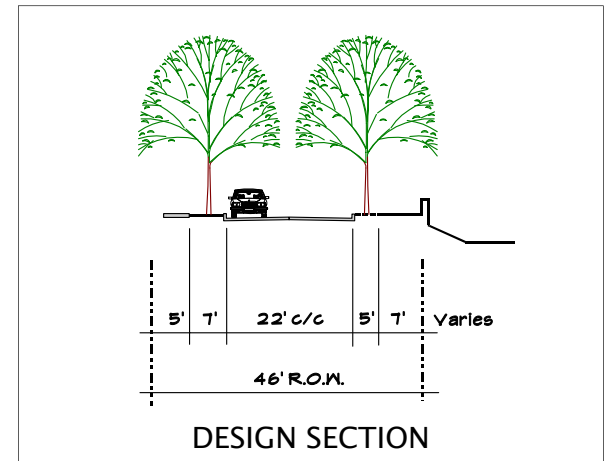
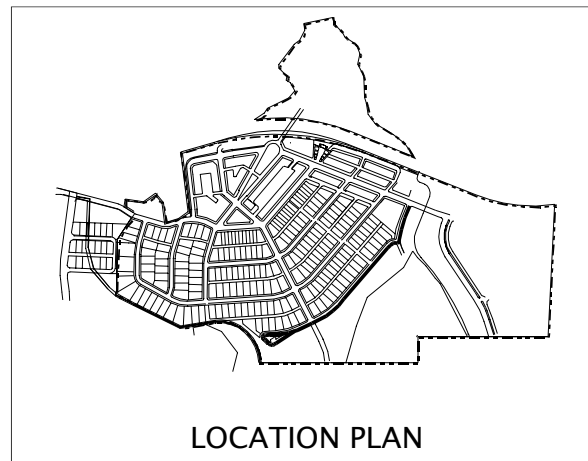
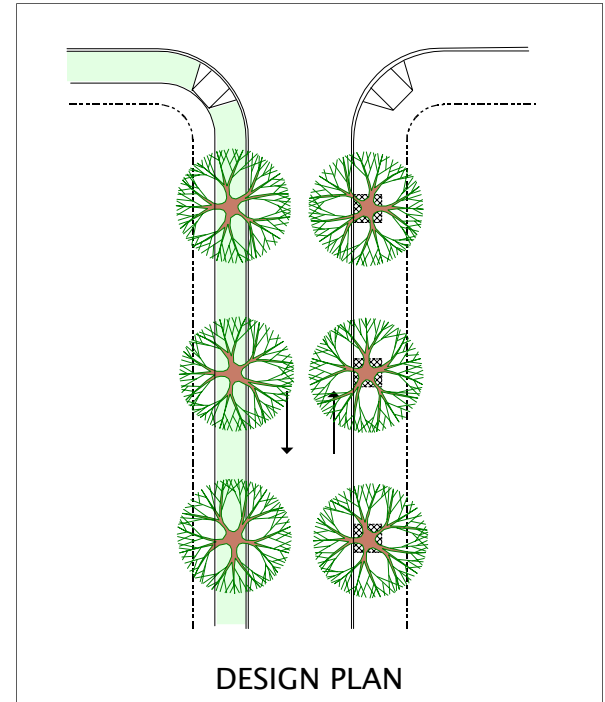
2.1.2.6 VALLEYVIEW TERRACE

Valley View Terrace is a single-loaded neighborhood street that roughly parallels Sycamore Avenue. It is located and designed with the following specific intentions:

- ◆ Provide vehicular and pedestrian access to the lots along the easterly edge of the neighborhood;
- ◆ Provide a relative quiet, low-speed thoroughfare for these frontages, rather than fronting house directly onto the busier Sycamore Avenue;
- ◆ By its elevation above Sycamore Avenue, Valley View Terrace is intended to provide views of the Refugio Valley and the greenway of the Creek.
- ◆ The easterly side of the street, which has no lots abutting it, is intended to be developed as a prime pedestrian way connecting the Bayfront District to the north to Duck Pond Park to the south. This will allow pedestrians to move from the Bay to the Park, and from there along the existing Duck Pond Trail (within the existing City easement along the south edge of the Neighborhood) without the need to walk along Sycamore Avenue.

DESIGN NOTES:

- ◆ Curbside parking is permitted on the west side of the street only.
- ◆ A pedestrian trail paved with decomposed granite or equal is provided along the east side of the street adjacent to the park.
- ◆ Parking is prohibited within 25 feet of corners to allow safe turning movements by occasional large vehicles.
- ◆ Street trees shall be maintained by the City. All other landscaping within the parkway strips shall be irrigated and maintained by owner of the adjoining property.
- ◆ District standard streetlights shall be provided along the northeast side of the street, spaced at approximately 60 feet on center. See Landscape Regulations for detailed information.
- ◆



2.1.2.7 NEIGHBORHOOD LANES

Neighborhood lanes are designed and located with the following intentions:

- ♦ Provide vehicular access to the rear of lots, freeing the facades of garage doors and driveways.
- ♦ Provide direct access to second residential units that are permitted within outbuildings.
- ♦ Allow development of residences on narrower lots than would be permitted if garages were accessed from the street.

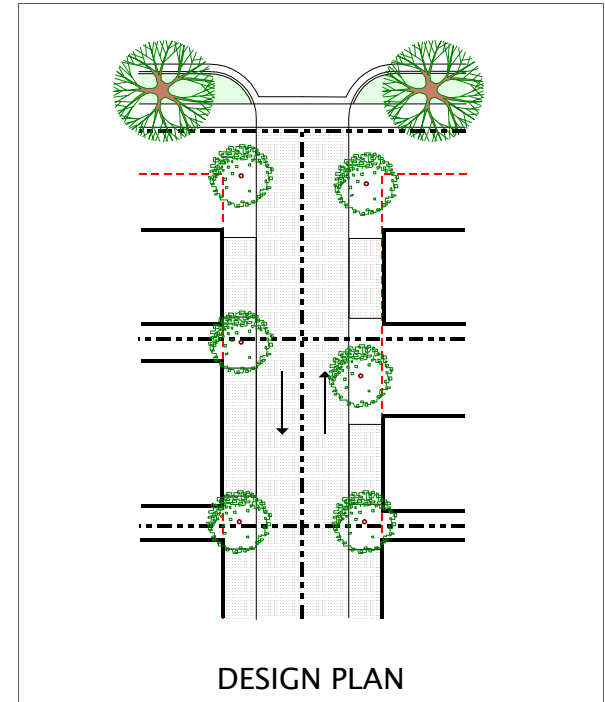
The lanes are public access easements on private land. An assessment maintenance district, formed by separate ordinance, shall maintain the pavement and any City utilities within the easement. The property owners shall maintain all other improvements, including but not limited to landscaping, lighting and fences.

Parking shall be prohibited within the 20-foot access easement.

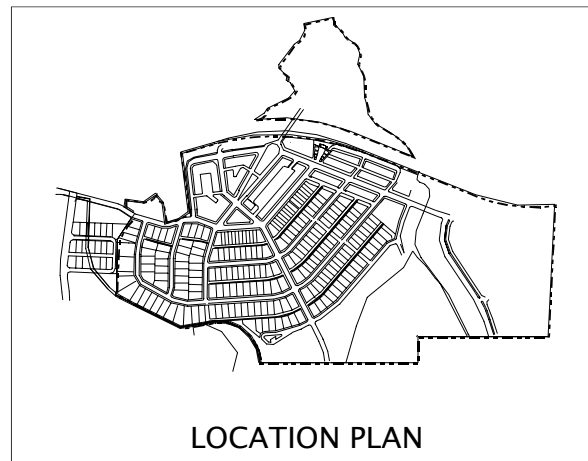
An recommended alternative cross section includes a central strip of asphalt pavement 14 feet wide, flanked by 3-foot strips of enhanced concrete pavement or turf block. If this section is provided the setback to the garage may be reduced from 7 feet to 5 feet, making the minimum clear dimension from building to building 30 feet rather than 34 feet.

DESIGN NOTES:

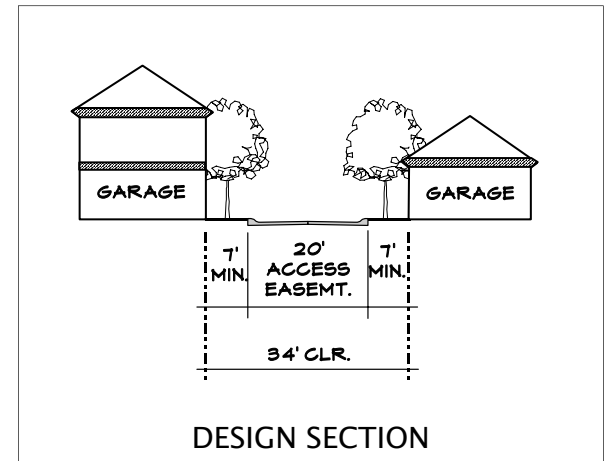
- ♦ Lanes shall be paved with asphalt or concrete. The pavement edges are to be finished with rolled concrete curbs, or equivalent device.
- ♦ Driveways to garages or open parking spaces on the lots shall not exceed 18 feet in width. Verges not paved with driveways shall be landscaped per the Landscape Regulations.
- ♦ Parking within 20-foot access easement shall be prohibited. Parking spaces flanking the lane shall be set back from the public access easement as required by the Urban Regulations for the lot.
- ♦ Down-directed light fixtures shall be mounted on the buildings or on poles by the property owners. See the Architectural Regulations.



DESIGN PLAN



LOCATION PLAN



DESIGN SECTION

2.1.2.8 MIDBLOCK LANES

Mid-block lanes are located and designed with the following intentions:

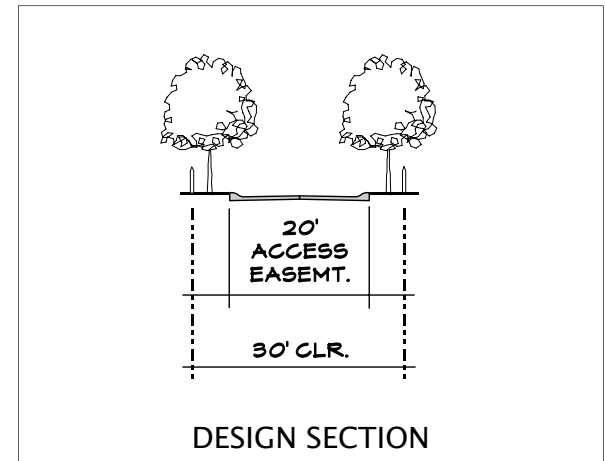
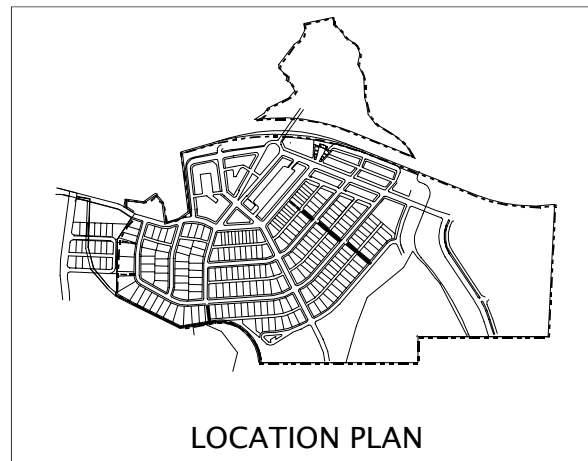
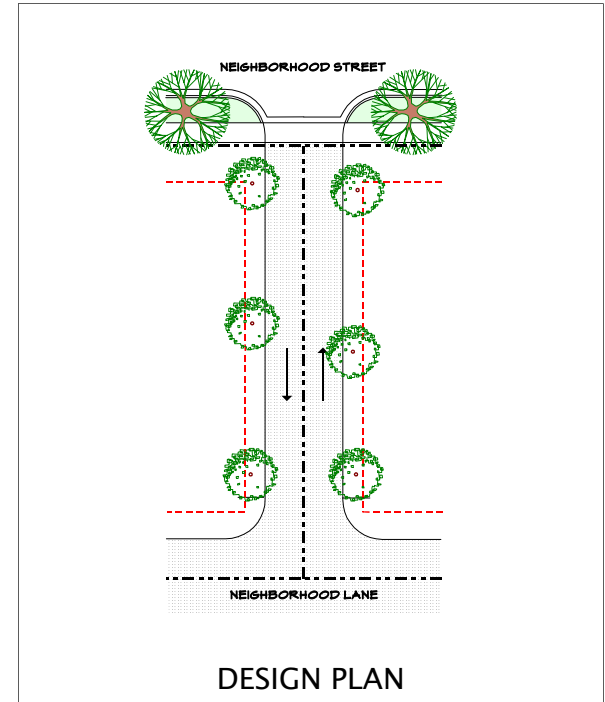
- ♦ Reduce the required driving distance from the street to a garage located on a neighborhood alley.
- ♦ Reduce the required walking distance within the Neighborhood, and to and from the Town Center District to the north.

The lanes are public access easements on private land. An assessment maintenance district, formed by separate ordinance, shall maintain the pavement and any City utilities within the easement. The property owners shall maintain all other improvements, including but not limited to landscaping, lighting and fences.

The 100 feet of the mid-block lane nearest Railroad Avenue shall be narrower than this typical section, for pedestrian access only. That section shall be 12 feet wide., with a concrete walk at the center and landscaping each side. The pedestrian passages that connect from to Talley Way to the west shall be similar, and include a concrete walk, stairs and landscaping.

DESIGN NOTES:

- ♦ Mid-block lanes shall be paved with asphalt or concrete. The pavement edges are to be finished with rolled concrete curbs, or equivalent device.
- ♦ Parking within the 20-foot access easement shall be prohibited.
- ♦ Verges shall be landscaped in accordance with the Landscape Regulations.
- ♦ Low-level lighting, mounted on fence posts, shall be provided by the adjacent property owners. See Architectural Regulations.



2.1.2.9 NEIGHBORHOOD EDGE ALLEYS

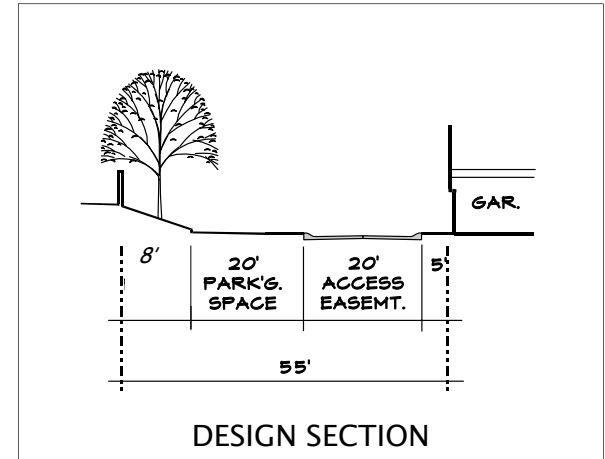
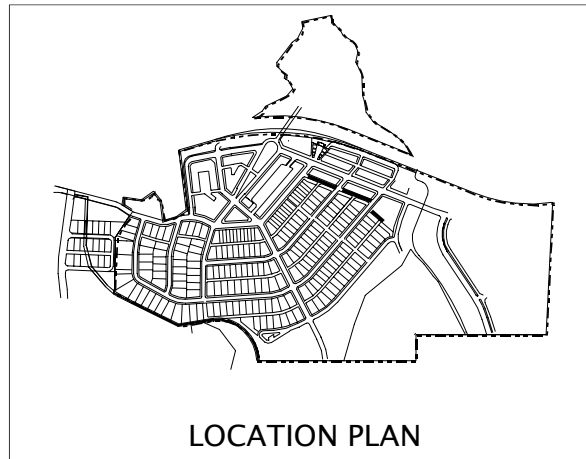
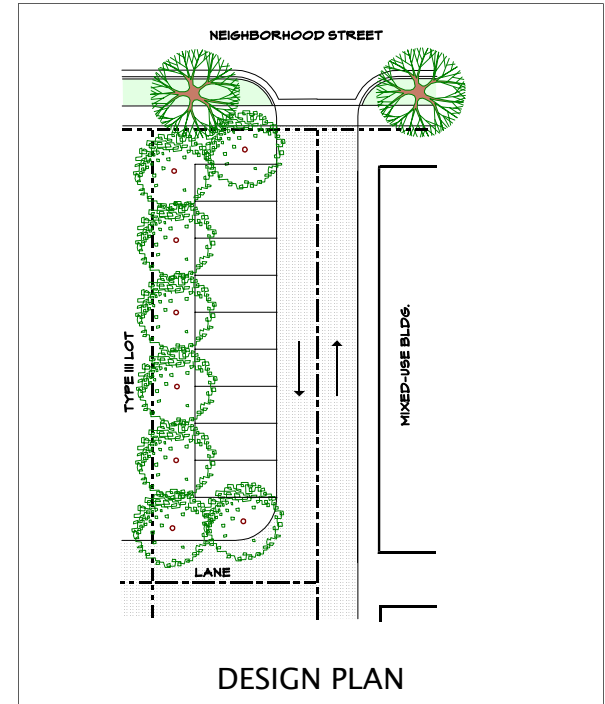
Alleys are provided at the north edge of the Neighborhood. Their purposes include:

- ♦ Providing access to and egress from the north ends of the Neighborhood Lanes.
- ♦ Providing convenient bicycle and pedestrian access to from the residences to the shops and offices along Bayfront Boulevard. Mid-block passages will be required in along Bayfront, such that direct access to Bayfront will be afforded pedestrians.

Providing access to parking in mixed-use buildings within the Bayfront District. Parking will be located on the north side of the alleys – within the ground floors of buildings that front onto Bayfront Boulevard – and outdoor on-site parking will be provided on the south side of the alley. It is intended that the adjoining lots of the Neighborhood will be at a higher elevation than these parking areas (by at least 2-3 feet) to minimize the impacts of the alley parking on the adjacent residences.

DESIGN NOTES:

- ♦ Parking spaces on both sides of the alley are at 90° to the alley. On the south side these are open spaces, and on the north they will be within ground floor garages of mixed-use buildings built in a future phase of development.
- ♦ A dense landscape buffer shall be provided at the south edge of the parking area south of the alley, to buffer the residences from light and glare. See the Landscape Regulations for the detailed planting requirements for parking lots.
- ♦ Down-directed light fixtures shall be mounted on the buildings along the north side of the alley. Lights on the south side shall be limited to low-level, bollard-mounted fixtures. See Architectural Regulations.



2.1.2.10 BAYFRONT BOULEVARD

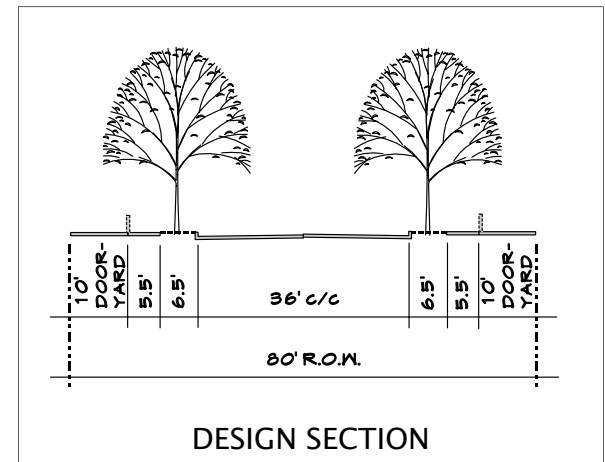
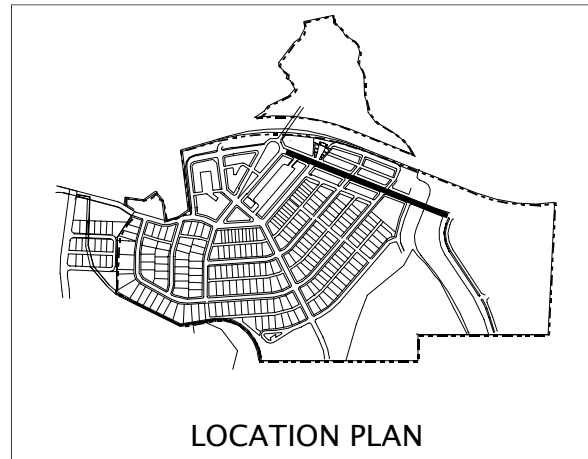
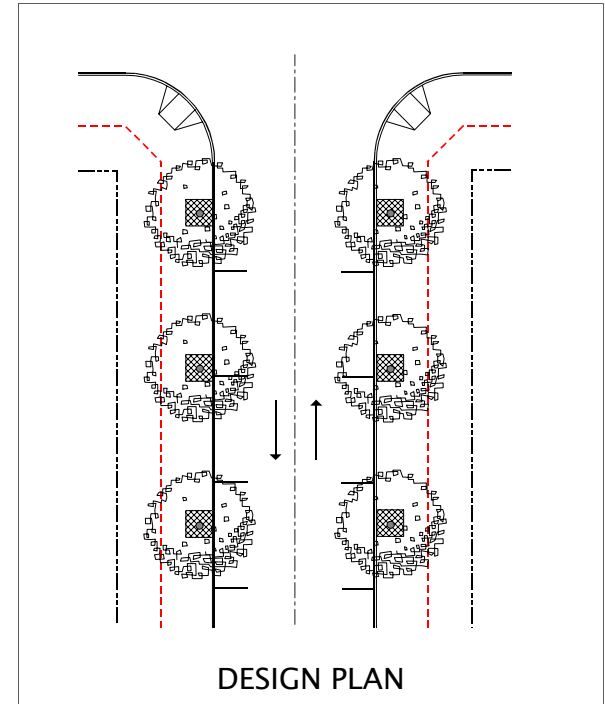
Bayfront Boulevard is planned to connect the northerly terminus of Sycamore Avenue to Railroad Avenue and the Historic Town Center. Bayfront will also provide access to the future Amtrak station and other uses to the east of Sycamore.

This Boulevard is intended to be urban in character, flanked by side sidewalks and multi-story, mixed-use buildings. It is detailed with the following specific intentions:

- ◆ Concentrating relatively high volumes of commercial and through traffic along frontages with commercial uses that will benefit from the traffic.
- ◆ Providing convenient access to the neighborhood streets to the south.
- ◆ Providing wide sidewalks suitable relatively high volumes of pedestrian traffic, and able to accommodate sidewalk dining and other business-related uses.

DESIGN NOTES:

- ◆ Parking spaces on both sides of the alley are at 90° to the alley. On the south side these are open spaces, and on the north they will be curbside parking. Curbside parking is provided on both sides of the street.
- ◆ Street trees shall be planted in tree grates within the sidewalk pavement.
- ◆ Dooryards may be provided at adjacent frontages. See Dooryard in Definitions. Such Dooryards are intended to define semi-public space for the use of the residents or businesses within the flanking buildings.
- ◆ Special double-headed streetlights with banner arms shall be provided along both sides of the street, spaced at approximately 60 feet on center. See Landscape Regulations for detailed information about streetlights and other urban design accessories.



2.0 OPEN SPACE MASTER PLAN

The community gathering places of the District are its public open spaces. A range of open space types punctuates the District – the open spaces attach to and enhance the circulation network. The General Plan and Zoning Ordinance set forth the following criteria to be met by open spaces within and surrounding the District:

- ♦ Reserve designated Waterfront areas for a mix of commercial, public, recreation and open space uses that are consistent with the General Plan, and preserve and enhance the characteristic meeting of the land and water.
- ♦ Usable open space is required to serve residents, visitors and employees;
- ♦ Public and conservation oriented open spaces should provide public access to natural, scenic and historic areas;
- ♦ Protect sensitive habitats of the shoreline area and Refugio Creek while providing trails and access points along the shoreline and creek edges.

Accordingly, the open space elements within the District include a range of natural and urban open spaces, which are located, sized and designed to promote and support a range of community uses. These spaces include active recreation parks, recreational greenways with pedestrian and bicycle trails, quiet recreation greens and playgrounds, and urban plazas and paseos. Each open space is designed to support public access and use, and to be compatible with adjacent land uses and building types.

The primary open spaces within the District are located on the map on the following page, and are described in detail in the Landscape Regulations. These open spaces include:

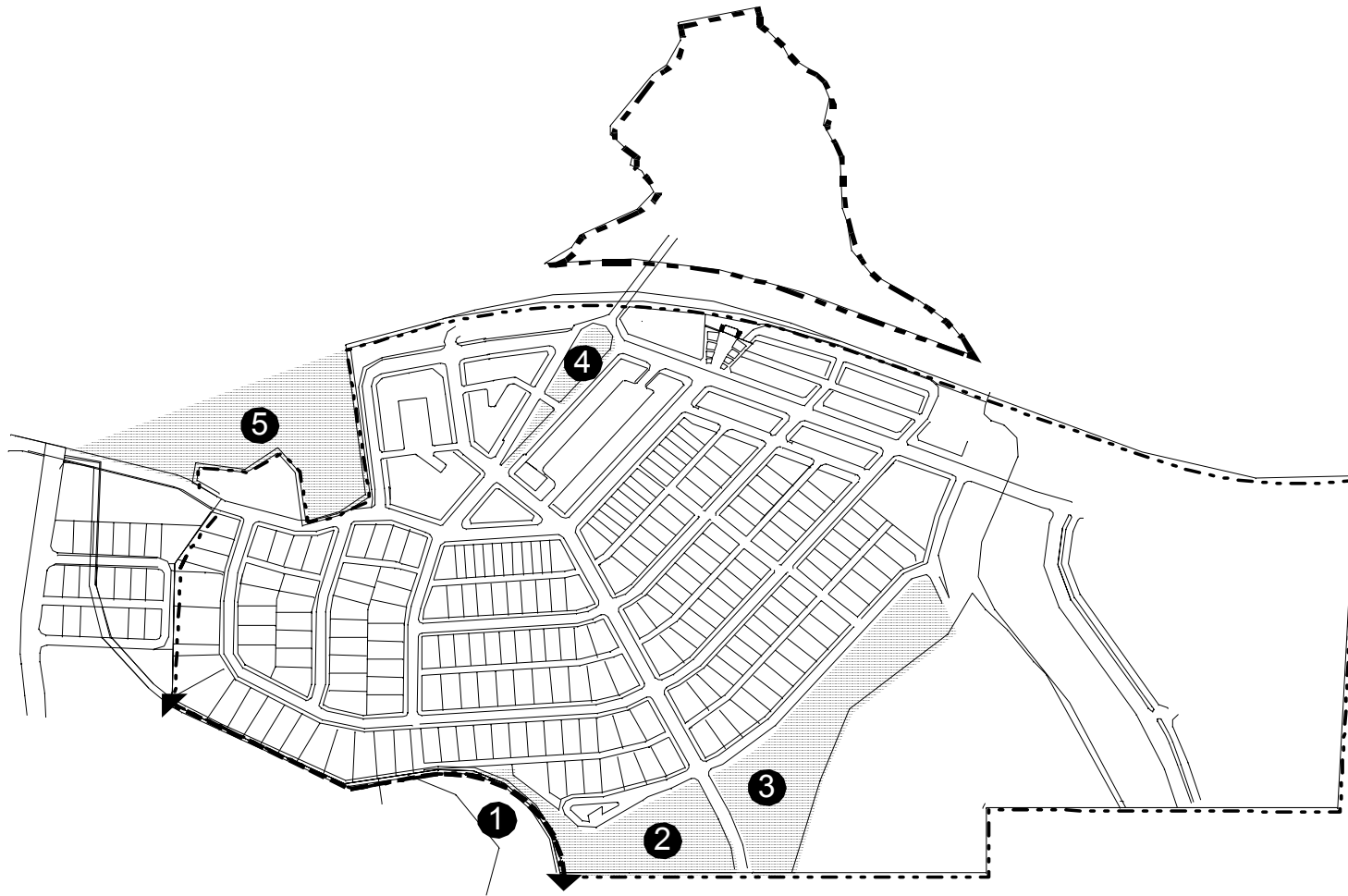
1. Duck Pond Trail: This existing trail follows the westerly and southerly boundaries of the Neighborhood.
2. Duck Pond Park: This new park is designed around the existing pond. The park is bounded on the west by the an existing pedestrian trail, on the south by an adjoining undeveloped property, on the east by the planned extension of Sycamore Avenue, and on the north by planned neighborhood thoroughfares. The landscaping of the park is intended to transition from a palette of native wetland plants around the margins of the pond, to useable turf areas adjacent to the abutting streets. Facilities within the park will include a play area for small children, picnic tables and other furnishings intended to make the park a comfortable place for informal play and family gatherings
3. Sycamore Park: This park is intended primarily as a buffer and visual open space between Sycamore Avenue and a planned neighborhood street. The park includes linear ponds that are designed to hold storm water runoff from the neighborhood prior to its discharge into Refugio Creek. The ponds are designed to be a visual amenity, similar in appearance to the existing Duck Pond to the south. The easterly frontage of the

adjoining neighborhood street is designed as a pedestrian route from the Bayfront District to Duck Pond Park

4. Town Green: The Town Green is the formal center of the Waterfront District. Since the initial adoption of this Master Plan, as the suggestion of the City and its Town Planning consultant, it has been relocated to lie entirely within the Historic Town Center. The character and design of the Town Green will be described in detail in a future amendment of this Master Plan that will include development standards and design guidelines for the Historic Town Center.
5. San Pablo Bay Park (State Parkland): The City will cooperate with the East Bay Regional Park District and the State of California to maintain this existing park land as an attractive natural open space. The San Pablo Bay Trail runs along the north edge of this park.

2.0 OPEN SPACE MASTER PLAN

OPEN SPACE MASTER PLAN



3.0 THE CODE

3.1 INTENT OF THE CODE

The purpose of the Waterfront District ("District") is to encourage and direct development in the District so that the goals of the City of Hercules General Plan and the purposes of the Zoning Ordinance are achieved. The District shall ensure the following:

- New buildings are compatible with each other and with the historic factory town character of the site;
- Commercial and flexible-use buildings be developed along specific street frontages to create lively, pedestrian-oriented public spaces;
- The needs of the automobile be accommodated but not at the expense of the needs of the pedestrian;
- The permitting process be simplified and facilitated to encourage realization of the Plan.
- The design of all buildings, site improvements, signs, and other appurtenances within the District shall be regulated and approved in accordance with the provisions of this section.

The design of all buildings, site improvements, signs, and other appurtenances within the District shall be regulated and approved in accordance with the provisions of this section.

3.2 ORGANIZATION AND USE OF THE CODE

The Urban Regulations below define specific "Building Types" that regulate the design and use of buildings within the District. The Urban

Regulations, together with the Architectural and Landscape Regulations that follow them, constitute the development standards and design guidelines for all development in the District.

Permitted Land Uses: Please refer to the Regulating Plan on page 22 to identify the permitted Building Types for a particular lot. The legend identifies the boundary of the Waterfront District and assigns to each lot a building type or types. The permitted uses within the buildings are identified in the Urban Regulations for each Building Type, and defined in the Definitions section of this Code. Then refer to the Urban Regulations for that Building Type and to the Definitions to identify the uses permitted within buildings on the lot.

Development Standards: Please refer to the corresponding page(s) of the Urban Regulations, pages 21 to 46 to identify the development standards for the individual building type(s). These pages contain the requirements for development of any lot in the Plan area. Also, please refer to the Definitions section for information regarding permitted uses and permitted elements of construction.

Architectural Design Criteria: Please see the Architectural Regulations on pages 51 to 78 to identify the permitted materials, configurations and methods of construction. Methods, materials and forms prescribed in the Code are standard. All others are prohibited.

Landscape Design Criteria: Please see the Landscape Regulations on pages 77 to 85 to identify the permitted landscape and hardscape materials,

methods and designs. Methods, materials and forms prescribed in the Code are standard. All others are prohibited.

3.3 ADMINISTRATION OF THE CODE

This Master Plan has been approved as a Planned Development Plan for the District. Subsequently public and private improvements within the District may be approved ministerially by the Director, upon review of the proposed construction and the determination that the proposal is consistent with the intent and regulations of this Master Plan.

Prior to the construction within the District of any new building, addition to an existing building, alteration of the exterior appearance of an existing building or any site improvements, the City of Hercules shall perform a Community Development Director Design Review, as provided in Chapter 42 of the Zoning Ordinance. Specific alteration improvements requiring a District Permit include, but are not limited to: additions to existing buildings, exterior signage, interior signage visible from the street, repainting of exterior surfaces, replastering of existing stucco surfaces, addition or replacement of windows or doors, exterior lighting, landscaping, sidewalks, parking areas.

Applications for Community Development Director Design Review are submitted to the Director. The Director will review the application and advise the applicant in writing that the application is complete or that additional materials or information are required.

Upon determination of the application's

3.0 THE CODE

completeness, the Director will complete the review and take an action within 10 (ten) working days. The Director's action may be to approve, approve with conditions, deny, or continue the project for redesign. The decision of the Director may be appealed to the Planning Commission.

Affordable Housing: An important objective of the City's General Plan is that housing types affordable by a wide range of households be provided within the City, and within each District. This District Master Plan includes a range of housing types and sizes intended to achieve this objective.

Within the Neighborhood – Phase I of the District development – in addition to range of lot and house sizes, accessory dwelling units are permitted, allowing a second small dwelling on the same lot with a primary residence. Such units are inherently relatively affordable due to their size and design, and their location on Lanes rather than Streets. The size of such second units will range from 400 to 700 s.f. These dwellings may be efficiency or studio apartments, or one-bedroom apartments. They may be attached to or detached from the principal residence, must be configured to function as an independent dwelling unit, and must be provided with a dedicated off-street parking space. These units may only be built on lots served by Lanes.

These second units will not be rent or price controlled. If required to meet the City's affordable housing requirements, the Bixby Company will provide a suitable lot on Bayfront

Boulevard or elsewhere within the District, will and enter into an agreement with a not-for-profit entity to construct and operate permanently affordable rental housing that meets the City's definitions of affordability.

3.4 GENERAL

1. This Code consists of the Regulating Plan and the Urban, Architectural and Landscape Regulations.
2. The Code shall determine the design of all private buildings on lots as designated on the Regulating Plan. The design of Civic Buildings shall not be limited by to these Regulations, because these buildings are to be unique and distinct from the retail, residential or general commercial buildings. Civic buildings are subject to review by and recommendation from the Director and approval by the Planning Commission. It is intended that the architectural quality of Civic Buildings exceed the general standard for commercial and residential buildings within the District.
3. Materials, construction methods, forms and colors prescribed in the Code are standard. All others are prohibited.
4. All building and site development plans shall be submitted to and reviewed by the Director for conformity to this Code.
5. The provisions of these Regulations, when in conflict, shall take precedence over the Zoning Ordinance.
6. The provisions of the Uniform Building Code, when in conflict, shall take precedence

over the provisions of these Regulations. Except that the Building Official may, in the case of buildings of recognized historical merit, invoke the provisions of the State of California Alternate Building Code.

3.5 DEFINITIONS

Terms used throughout these regulations shall take their commonly accepted meaning unless defined herein. When there are conflicts between the definitions herein and definitions as provided in the Zoning Ordinance, these shall take precedence.

Artisanal Use: The manufacture and sale of artifacts within an enclosed structure utilizing only hand-held and/or table-mounted electrical tools.

Civic Premises: Premises used primarily for public gatherings and displays administered by non-profit governmental, cultural, education, and religious organizations.

Director: The Community Development Director of the City of Hercules, or his duly appointed representative

Dooryard: A yard between the street and a building, raised at least 18 inches above the grade of the sidewalk adjacent, or bounded by a garden wall, between 18 inches and 36 inches in height built on the frontage line(s).

Facade: The vertical surface of a building that is set parallel to a Frontage Line and facing a street. (Building walls containing garage doors are not classified as Facades, and may not be located on lots where Facades are permitted and/or required by these regulations.)

3.0 THE CODE

Frontage Line: The property lines of a lot fronting a Street, Avenue or Mid-block Lane.

Front Porch: A roofed structure, that is not enclosed, attached to the facade of a building.

Height: A limit to the vertical extent of a building that is measured in number of stories. Height limits do not apply to masts, belfries, clock towers, chimney flues, water tanks, elevator bulkheads, and similar structures, which may be of any height approved by the Director.

Home Occupation: Residential premises used for the transaction of business or the supply of professional services. Home occupation shall be limited to the following: agent, architect, artist, broker, consultant, draftsman, dressmaker, engineer, interior decorator, lawyer, notary public, teacher, and other similar occupations, as determined by the Director. Such use shall not simultaneously employ more than 1 person in addition to residents of the dwelling. The total gross area of the home occupation use shall not exceed 25 percent of the gross square footage of the residential unit. The home occupation use shall not disrupt the generally residential character of the neighborhood. The Director shall review the nature of a proposed home occupation use at the time of review of a business license for such use, and may approve, approve with conditions, continue or deny the application.

Net Floor Area: The enclosed area of a building, excluding unglazed porches, arcades and balconies.

Outbuilding: A separate building in addition to, and in the rear yard of the principal building,

which is a maximum of 2 stories and has a maximum net floor area of 700 square feet per story, and not more than 1 bedroom.

Prohibited Uses: The following are examples of uses not permitted anywhere within the District: animal hatcheries; boarding houses; chemical manufacturing, storage, or distribution; any commercial use in where patrons remain in their automobiles while receiving goods or services, except service stations; enameling, painting, or plating of materials, except artist's studios; kennels; the manufacture, storage, or disposal of hazardous waste materials; mini-storage warehouses; outdoor advertising or billboards; packing houses; prisons or detention centers, except as accessory to a police station; drug and alcohol treatment and rehab centers; thrift stores; soup kitchens and charitable food distribution centers; sand, gravel, or other mineral extraction; scrap yards; tire vulcanizing and retreading; vending machines, except within a commercial building; uses providing goods or services of a predominantly adult-only or sexual nature, such as adult book or video stores or sex shops; and other similar uses as determined by the Director.

Parking Spaces: Parking spaces shall be a minimum of 9 feet x 19 feet, except that in off-street parking lots of more than 10 spaces, up to 20 percent of the spaces may be a minimum of 8 feet x 16 feet. The parking requirement may be accommodated on the lot and on the street on the corresponding frontage, or on another site by way of a shared parking proposal approved by the Director. Pairs of on-site parking spaces for

use by employees of a single business, or for use by residents of a single dwelling unit, may be provided in tandem configuration (one behind the other) when approved by the Director.

Residential: Premises used primarily for human habitation. Units shall not be less than 400 square feet in net area.

Setback: The mandatory distance between a property line and a building or appurtenance. This area must be left free of structures that are higher than 3 feet excluding Streetwalls, except as noted in the Urban Regulations.

Shared Parking: Any parking spaces assigned to more than one use, where persons utilizing the spaces are unlikely to need the spaces at the same time of day.

Story: A habitable floor level within a building, no more than 12 feet high from floor to ceiling. Individual spaces, such as lobbies and foyers may exceed one story in height.

Streetwall: An opaque freestanding wall built on the required setback line with the purpose of masking parking from the street. Streetwalls shall be between 4 and 5 feet in height, made of material matching the adjacent building, or be a continuous maintained hedge of the stated height. Streetwalls may have openings to allow automobile access and shall have openings for pedestrian access, which may be gated.

Terminated Vistas: A building or portion thereof designated to terminate a view through or along a street centerline.

3.6 URBAN REGULATIONS

The General Plan and Zoning Ordinance intend that development within the District be in the form of a true mixed-use district, with a range of office, retail, live-work and residential uses mixed together in a historic town pattern.

Conventional zoning ordinances are based on the notion of segregating land uses into separate areas, and are not able to effectively direct and regulate the planning, design and operation of a mixed-use district. Accordingly, this Code has been prepared for that purpose.

Because the uses within the buildings in the District are flexible, and subject to change over time, a limited range of permitted “Building Types” are defined by this Code. These Types have been collected and codified from existing districts within the Bay Area that have proven over time to be best able to accommodate a variety of uses and to successfully adapt as use patterns change in response to economic shifts.

The Regulating Plan defines which Building Types may be constructed within the blocks and on the lots within the District. The Regulating Plan, in conjunction with the Circulation and Open Space Master Plans, forms several distinct planning sub-areas that are seamlessly connected to one another by District thoroughfares.

The single family Neighborhood is the first of these planning sub-areas. It is characterized by narrow, tree-lined streets and traditional houses with front porches. Only Type I buildings are permitted in the Neighborhood, and a number of specific subtypes are defined in the following pages. These “Building Types” define the

location and configuration of the buildings, and are not the same as architectural “Styles” which are defined in the Architectural Regulations.

Additional planning sub-areas lie to the north and east of the Neighborhood. These include the Historic Town Center and the Bayfront District to the north, and mixed-use districts with a range of employment and residential uses to the east of Sycamore Avenue. These sub-areas will be described in subsequent drafts of this Master Plan, and developed in subsequent phases of the build-out of the Master Plan. Additional Building Types will be defined to guide development within those sub-areas.

The Building Types within the Neighborhood are defined on the following pages. They are:

Type I-A: A one- or two-story house intended for a lot 40 feet or wider, which is served a Lane. The house must be located in the area shown in section 3.6.1.1, and the garage may be attached to or detached from the house. Access to the garage must be from the Lane at the rear of the lot, and the garage and any outdoor parking must be in the area indicated in section 3.6.1.3. A front porch is encouraged, and may encroach into the front yard setback area, as shown in section 3.6.1.2.

Type I-B: A one- or two-story side-porch house intended for a relatively narrow lot that is served a Lane, providing vehicular access to the rear of the lot. The lot must be 40 feet or wider. The house must be located in the area shown in section 3.6.2.1, and the garage may be attached to or detached from the house. Access to the

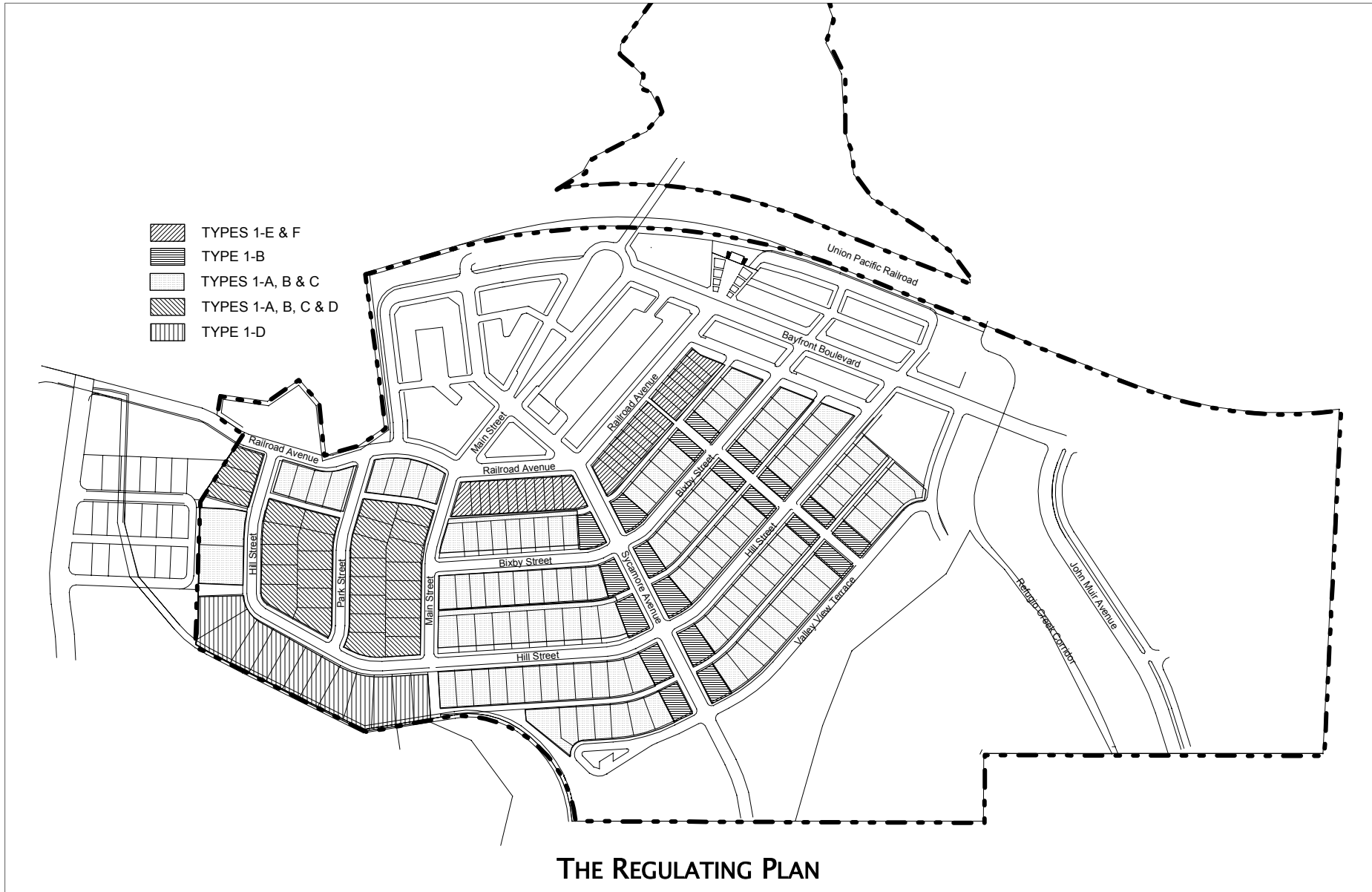
garage must be from the Lane at the rear of the lot, and parking must be in the area indicated in section 3.6.2.3. A side porch is required. If a sideyard use easement is recorded between adjacent lots of this type, no sideyard fence is required and the allowable lot coverage is increased.

Type I-C: A one- or two-story house intended for a lot 50 feet or wider, which is served a Lane. The house must be located in the area shown in section 3.6.3.1, and the garage may be attached to or detached from the house. Access to the garage must be from the Lane at the rear of the lot, and the garage and any outdoor parking must be in the area indicated in section 3.6.3.3. A front porch is encouraged, and may encroach into the front yard setback area, as shown in section 3.6.3.2.

Type I-D: A one- or two-story house intended for a lot 55 feet or wider, which is not served a Lane. The house must be located in the area shown in section 3.6.4.1, and the garage may be attached to or detached from the house. Access to the garage is provided by a narrow drive from the street. The garage and any outdoor parking must be in the area indicated in section 3.6.4.3. A front porch is encouraged, and may encroach into the front yard setback area, as shown in section 3.6.4.2.

Types I-E & I-F: Two-story houses on lots between 24 and 30 feet wide, on lots served by rear access lanes. Garages may be attached or detached. Grade changes between adjacent lots shall be retained by building footings.

3.6.0 THE REGULATING PLAN

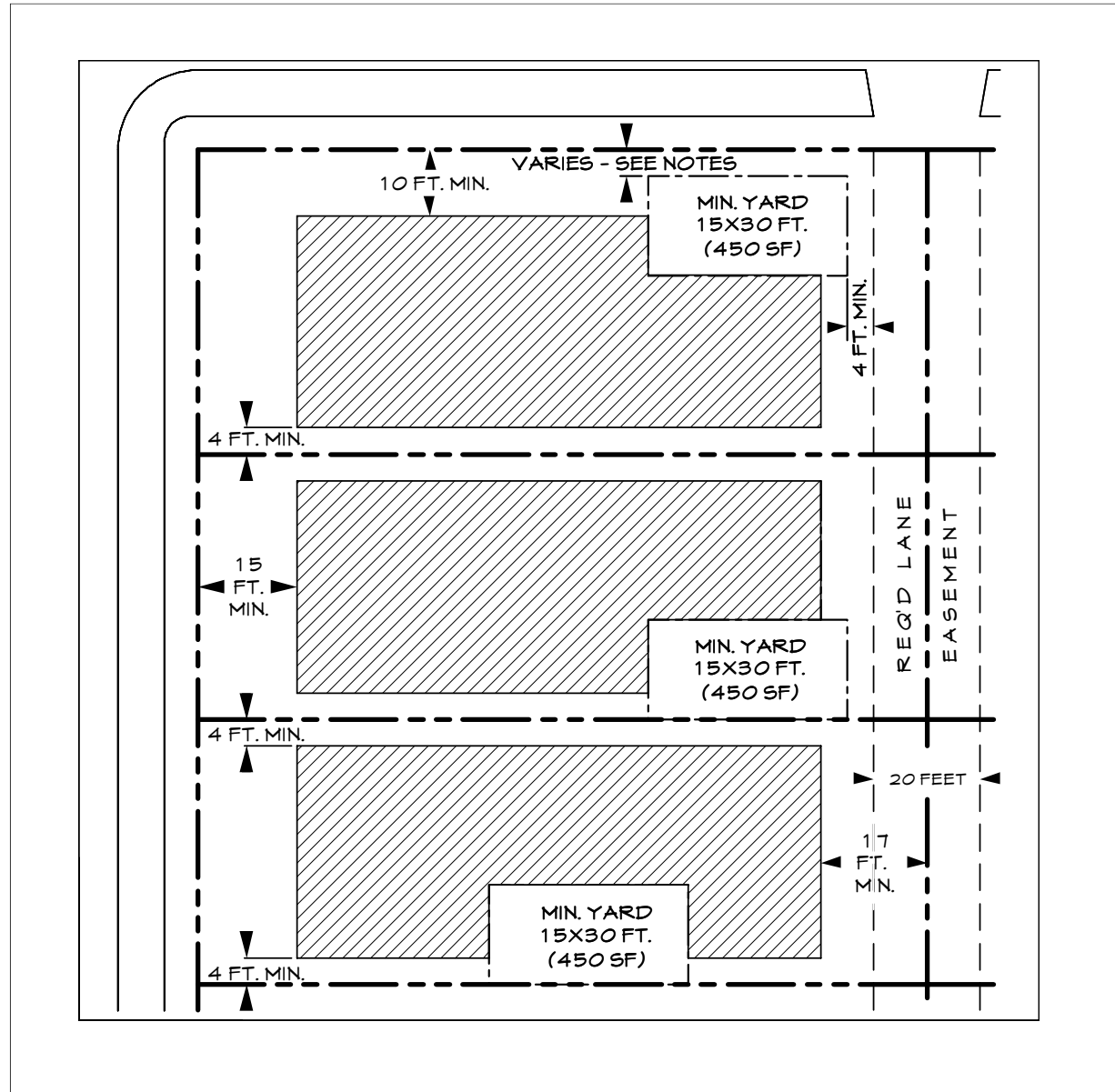


THE REGULATING PLAN

3.6.1 BUILDING TYPE I-A

3.6.1.1 BUILDING PLACEMENT

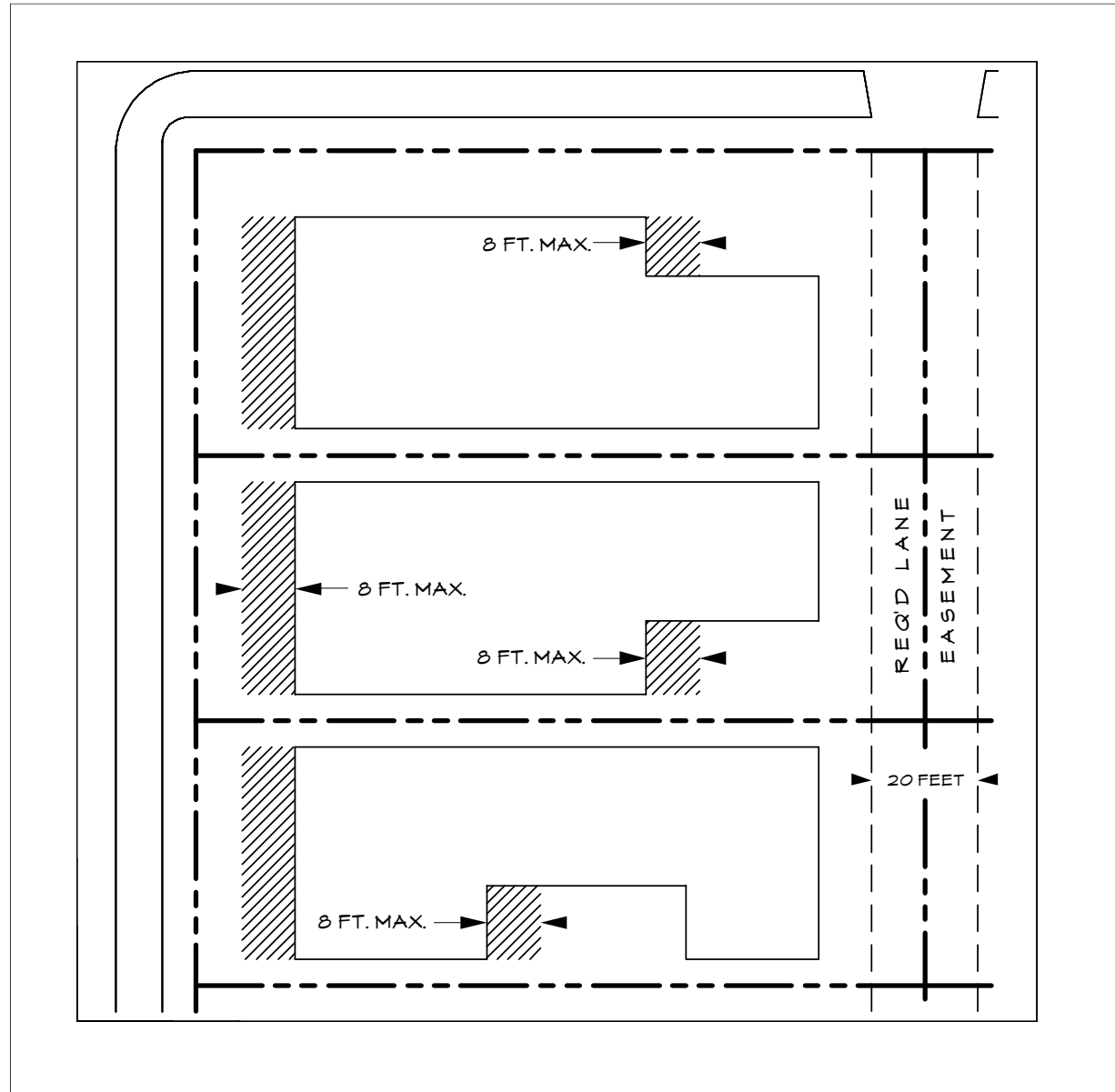
1. Buildings shall be set on lots relative to property lines as shown here.
2. Building Facades shall extend along the Frontages as shown here.
3. Minimum required enclosed yard areas shall be provided as shown here.
4. Required enclosed yards shall be surrounded by fences or garden walls conforming to the requirements of the Architectural Regulations. See Building Elements, below, for additional requirements for wall and fences.
5. This building type may be built only on lots that are provided with alley access, and are between 40 and 49 feet in width.



3.6.1 BUILDING TYPE I-A

3.6.1.2 BUILDING ELEMENTS

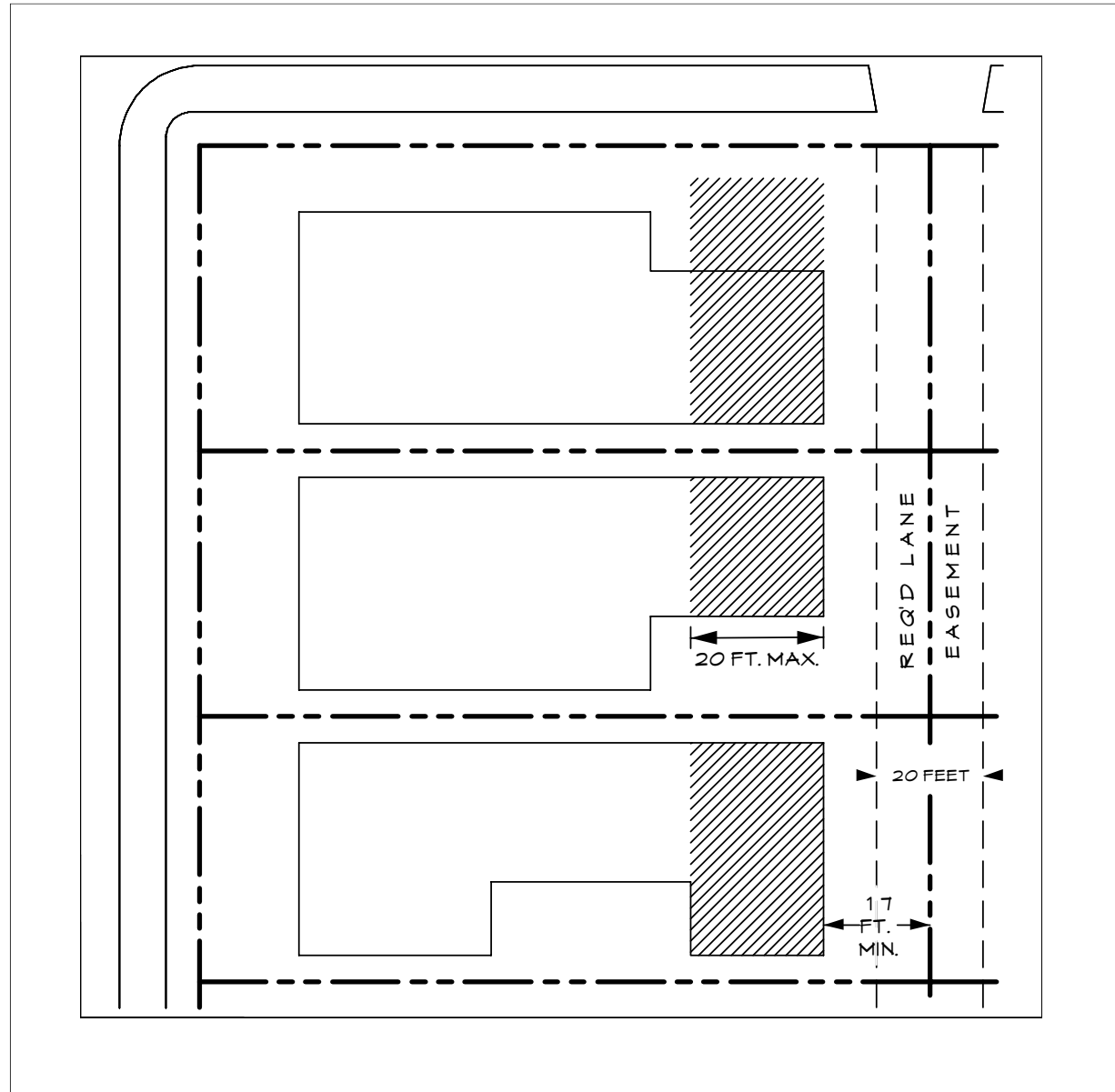
1. Front porches may encroach up to 8 feet into the front yard setback area.
2. Front porches shall be a minimum clear area of 7 feet by 12 feet.
3. Stoops may encroach up to 6 feet into the Front Yard setback area.
4. Pergolas and back porches attached to the primary building may encroach up to 8 feet into the required enclosed yard area. Such encroachments shall not exceed 12 feet in width.
5. Gazebos may be constructed within required enclosed yard areas, provided that they be set back a minimum of 5 feet from an adjoining lot.
6. Chimneys may encroach up to 2 feet into Side Yard and Rear Yard setback areas. Roof overhangs may encroach up to 2 feet into any required setback area.
7. Walls and fences within 10 feet of a Frontage Line may be between 3 feet and 3 feet 6 inches in height. Walls and fences 10 or more feet from a frontage line may be up to 6 feet in height.



3.6.1 BUILDING TYPE I-A

3.6.1.3 PARKING

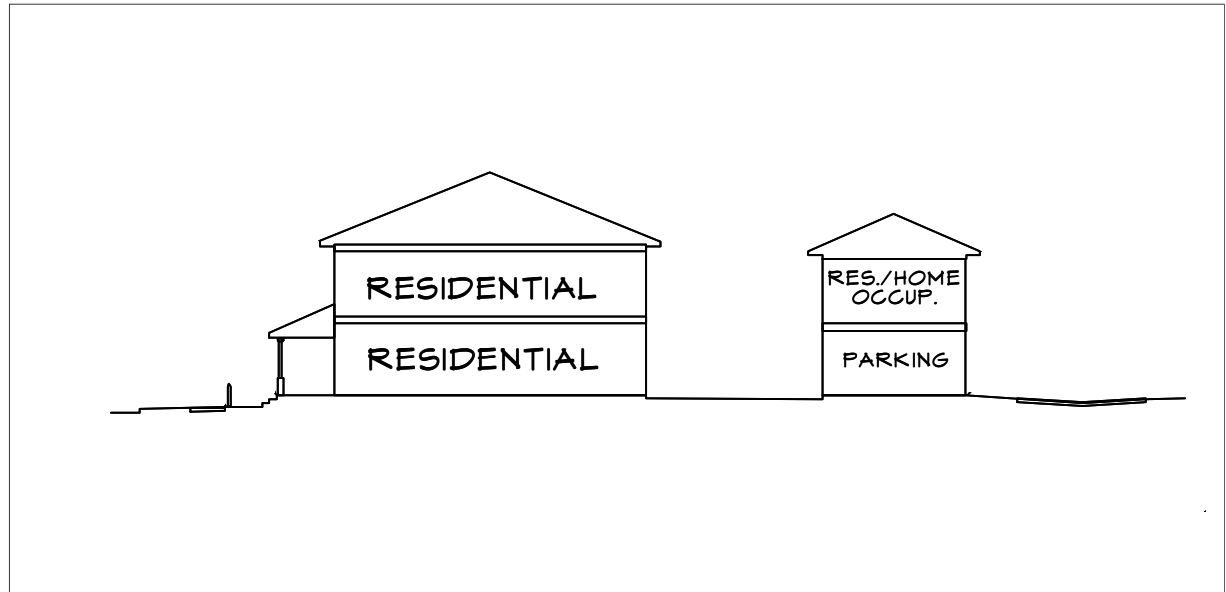
1. Off-street parking spaces shall be placed only within the areas shown here. Parking spaces shall not be less than 10 feet from the side-street frontage lines, not less than 4 feet from interior side property lines. Note that parking is only permitted within the rear 37 feet of each lot.
2. On-street parking spaces along the Frontage shall meet the requirement for guest parking.
3. Off-street parking spaces shall be no less than 9 feet by 19 feet.
4. 2 parking spaces within a garage shall be provided on each lot. 1 additional on-site parking space, which may be open or covered, shall be provided for an accessory dwelling (second unit) if present on the lot.
5. Trash containers and enclosures shall be located within the areas where parking is permitted.
6. Required enclosed yard areas shall be free of parking. As shown in the lower lot on the illustration here, if a third on-site parking space is provided, the required enclosed yard area must move closer to the front of the lot. This reduces the allowable footprint area of the house.



3.6.1 BUILDING TYPE I-A

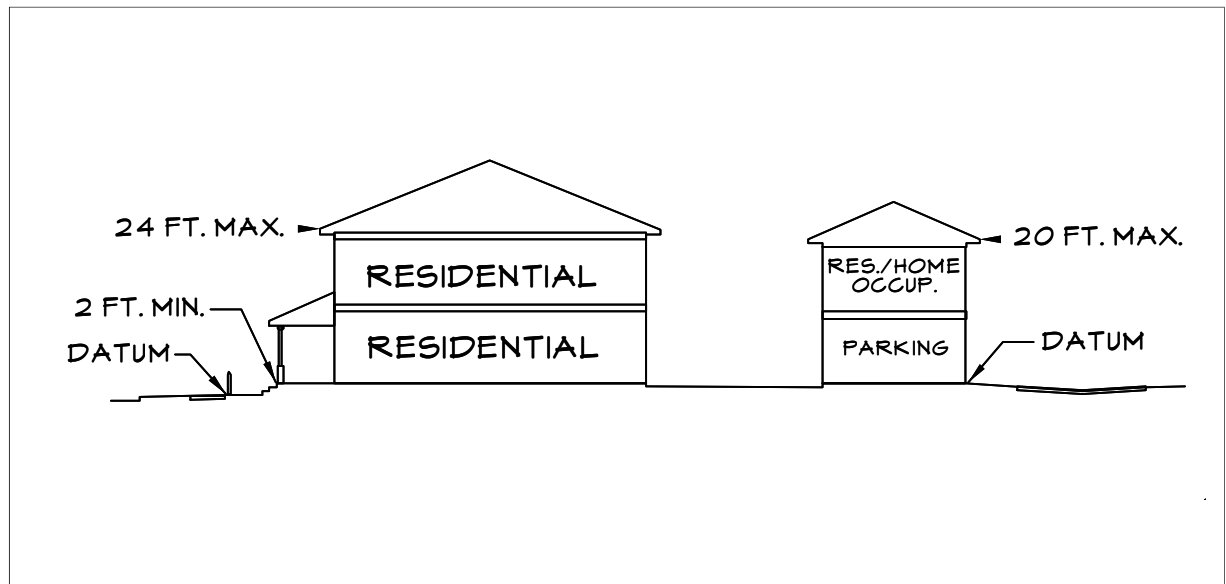
3.6.1.4 BUILDING USE

- 1. Uses permitted within the buildings shall be as shown here.
- 2. Home occupation uses shall be approved by the Director as part of the business license issuance process. See Home Occupation in Definitions.



3.6.1.5 BUILDING HEIGHT

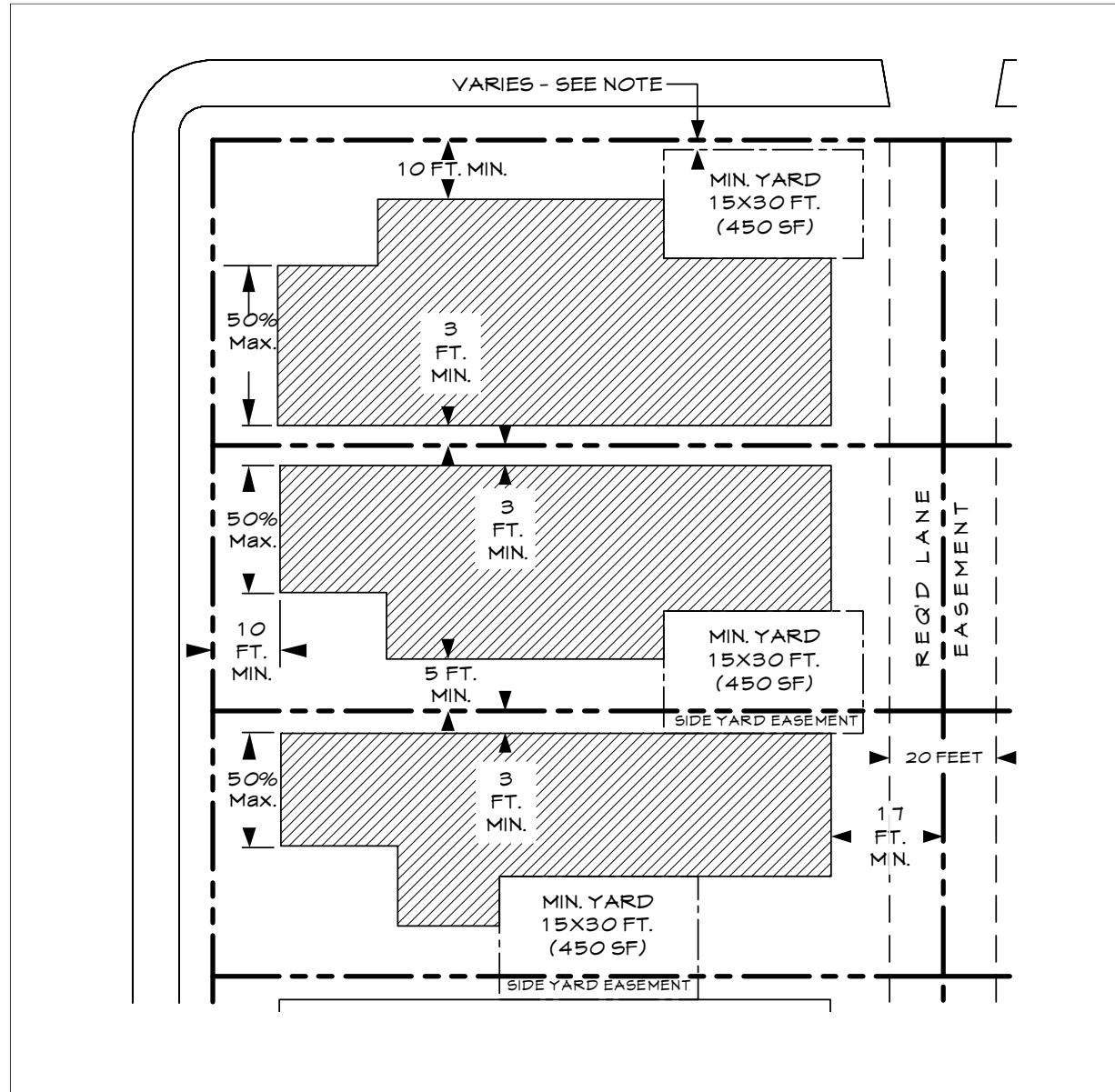
- 1. Maximum building heights shall be measured relative to the sidewalk elevation and a specified point as shown here.
- 2. The height of eaves and ridges for building additions shall substantially match the heights of the original building, as approved by the Director.
- 3. Note that the main floor of the house, and front porch if provided, shall be raised above the sidewalk as shown here.



3.6.2 BUILDING TYPE I-B

3.6.2.1 BUILDING PLACEMENT

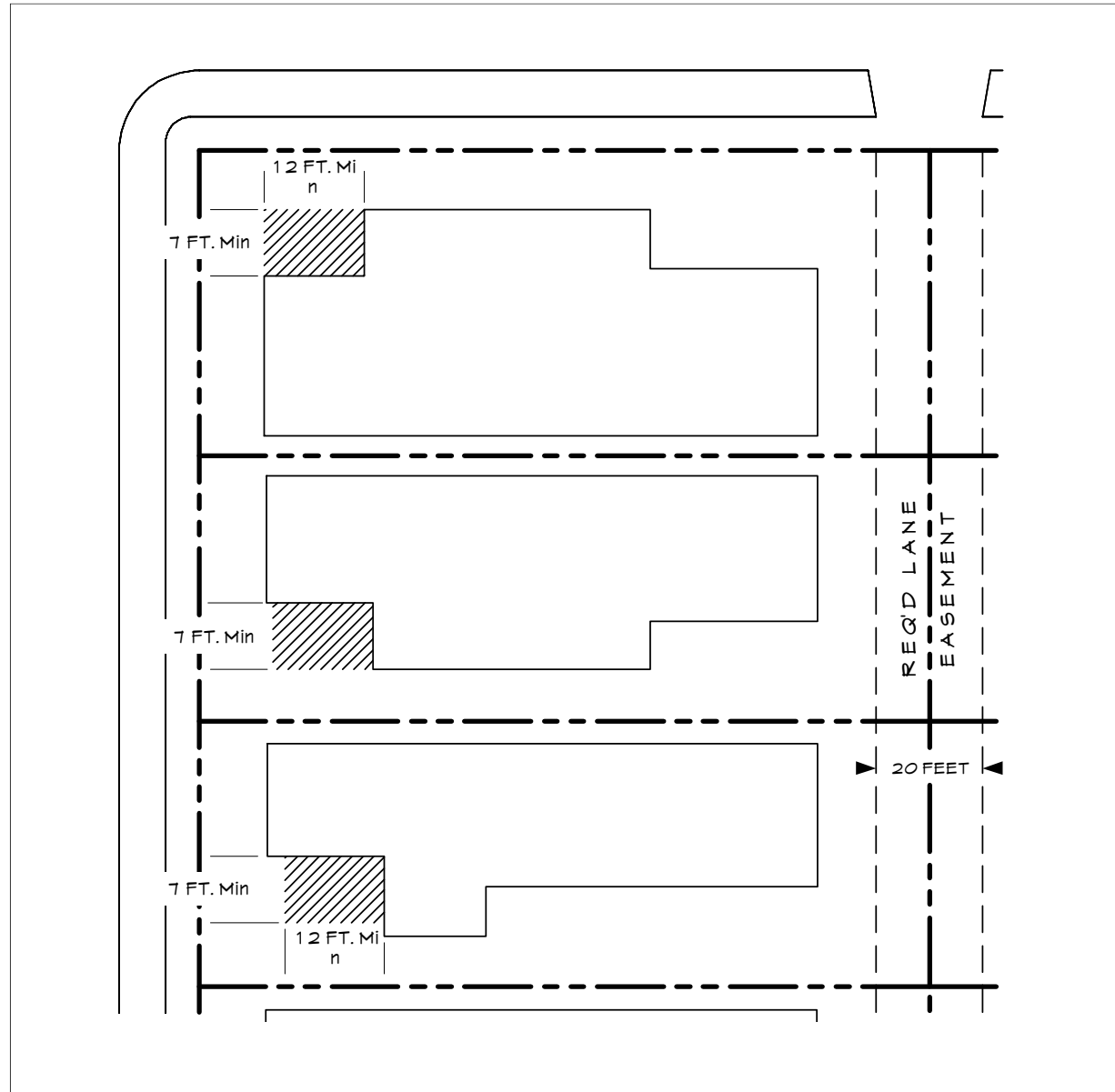
1. Buildings shall be set on lots relative to property lines as shown here.
2. Building Facades shall extend along Frontages as designated here.
3. Side yards with side porches shall be a minimum of 8 feet in width, measured from the house to a fence or building wall. This may include the 3-foot setback area of the neighboring lot if an irrevocable yard agreement is recorded between the properties. In such case, the yard shall be caused to drain onto the subject lot, and the side elevation of the neighboring house shall meet the requirements for Sideporch Houses as set forth in the Architectural Regulations.
4. Minimum required enclosed yard areas shall be provided as shown here. Required enclosed yards shall be surrounded by fences or garden walls conforming to the requirements of the Architectural Regulations. See Building Elements, below, for additional requirements for wall and fences.
5. Walls and fences within 10 feet of a Frontage Line may be between 3 feet and 3 feet 6 inches in height. Walls and fences 10 or more feet from a frontage line may be up to 6 feet in height.



3.6.2 BUILDING TYPE I-B

3.6.2.2 BUILDING ELEMENTS

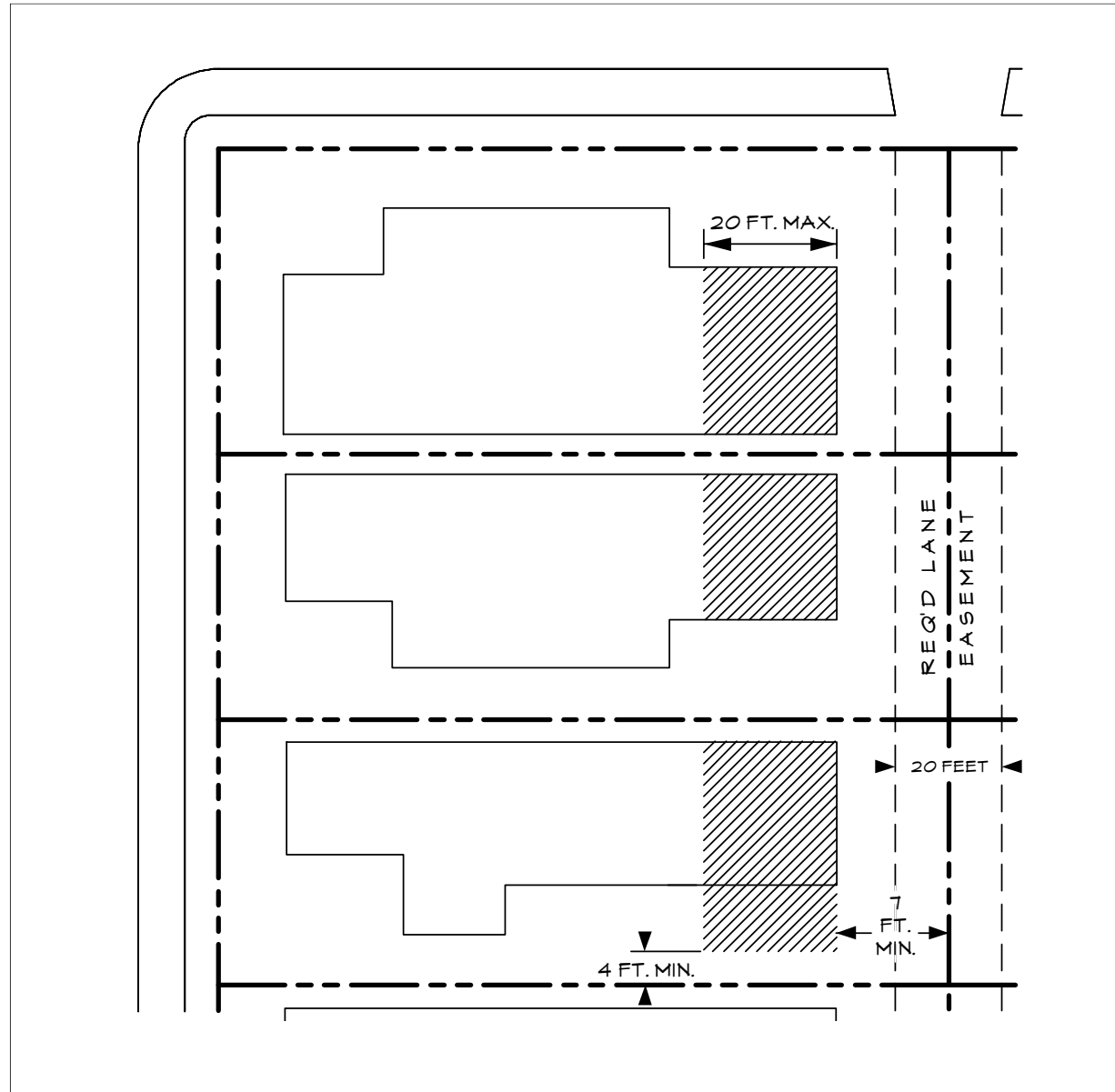
1. Side porches shall be located as shown here.
2. Porches shall be a minimum clear area of 7 feet by 12 feet.
3. Stoops may encroach up to 4 feet into the Front Yard setback area.
4. Pergolas and back porches attached to the primary building may encroach up to 8 feet into the required enclosed yard area. Such encroachments shall not exceed 12 feet in width.
5. Gazebos may be constructed within required enclosed yard areas, provided that they be set back a minimum of 5 feet from an adjoining lot.
6. Chimneys may encroach up to 1 foot into Side Yard and 2 feet into Rear Yard setback areas. Roof overhangs may encroach up to 2 feet into any required setback area.
7. Walls and fences within 10 feet of a Frontage Line may be between 3 feet and 3 feet 6 inches in height. Walls and fences 10 or more feet from a frontage line may be up to 6 feet in height.



3.6.2 BUILDING TYPE I-B

3.6.2.3 PARKING

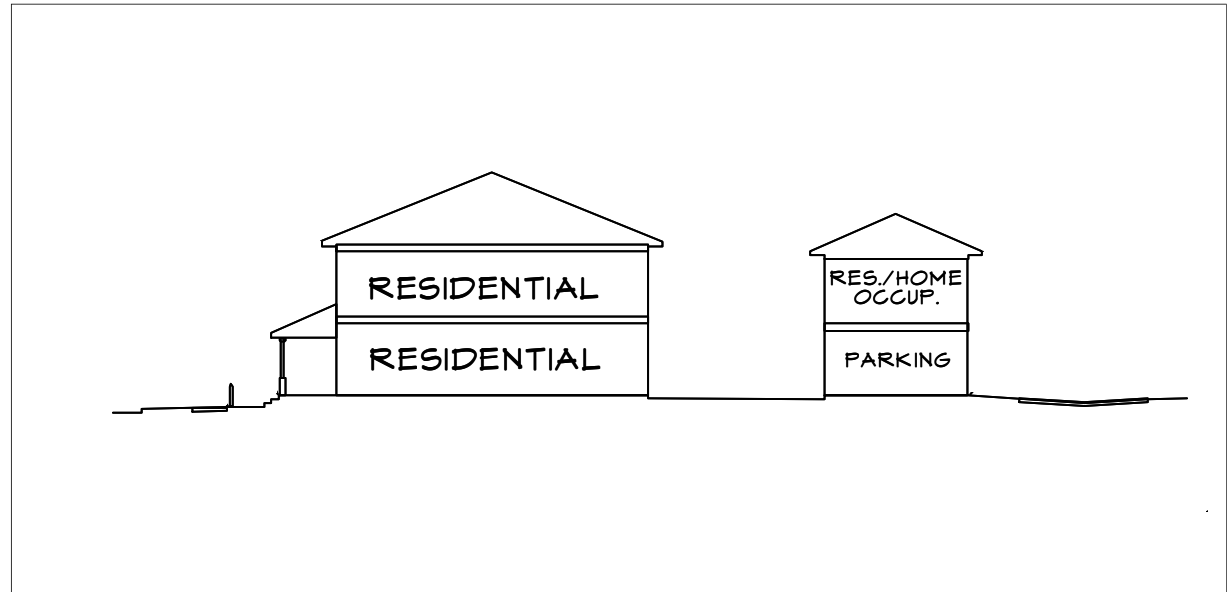
1. Off-street parking spaces shall be placed only within the areas shown here. Parking spaces shall not be less than 10 feet from the side-street frontage lines, not less than 4 feet from interior side property lines. Note that parking is only permitted within the rear 37 feet of each lot.
2. On-street parking spaces along the Frontage shall meet the requirement for guest parking.
3. Off-street parking spaces shall be no less than 9 feet by 19 feet.
4. 2 parking spaces within a garage shall be provided on each lot. Accessory dwelling units (second units) shall be provided with 1 additional on-site parking space, which may be open or covered.
5. Trash containers and enclosures shall be located within the areas where parking is permitted.
6. Required enclosed yard areas shall be free of parking. As shown in the lower lot on the illustration here, if a third on-site parking space is provided, the required enclosed yard area must move closer to the front of the lot. This reduces the allowable footprint area of the house.



3.6.2 BUILDING TYPE I-B

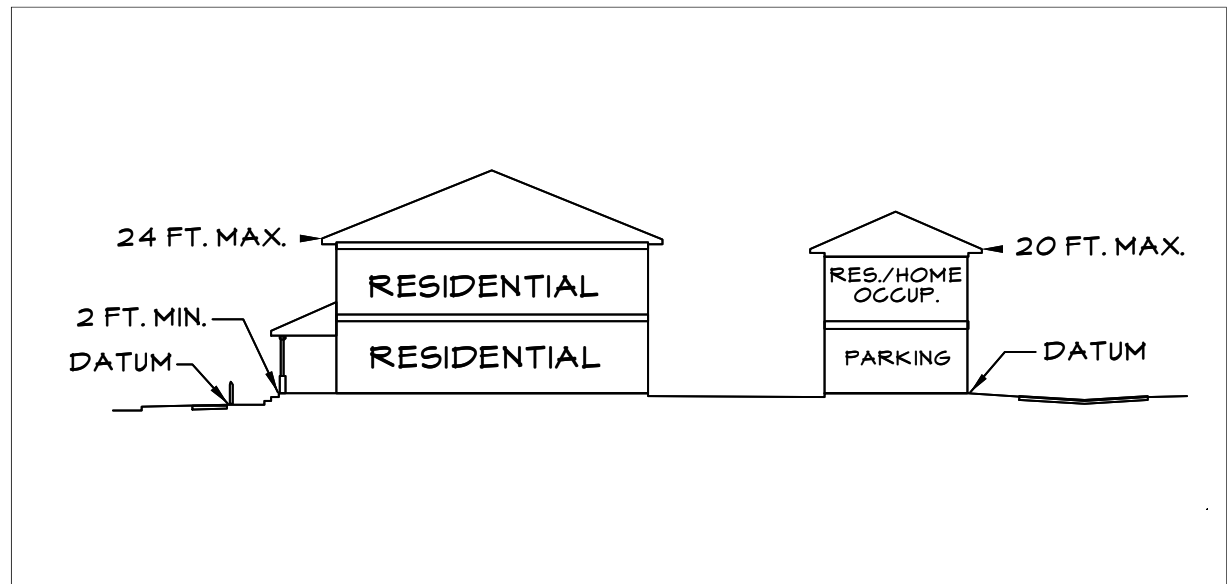
3.6.2.4 BUILDING USE

- 1. Uses permitted within the buildings shall be as shown here.
- 2. Home occupation uses shall be approved by the Director as part of the business license issuance process. See Home Occupation in Definitions.



3.6.2.5 BUILDING HEIGHT

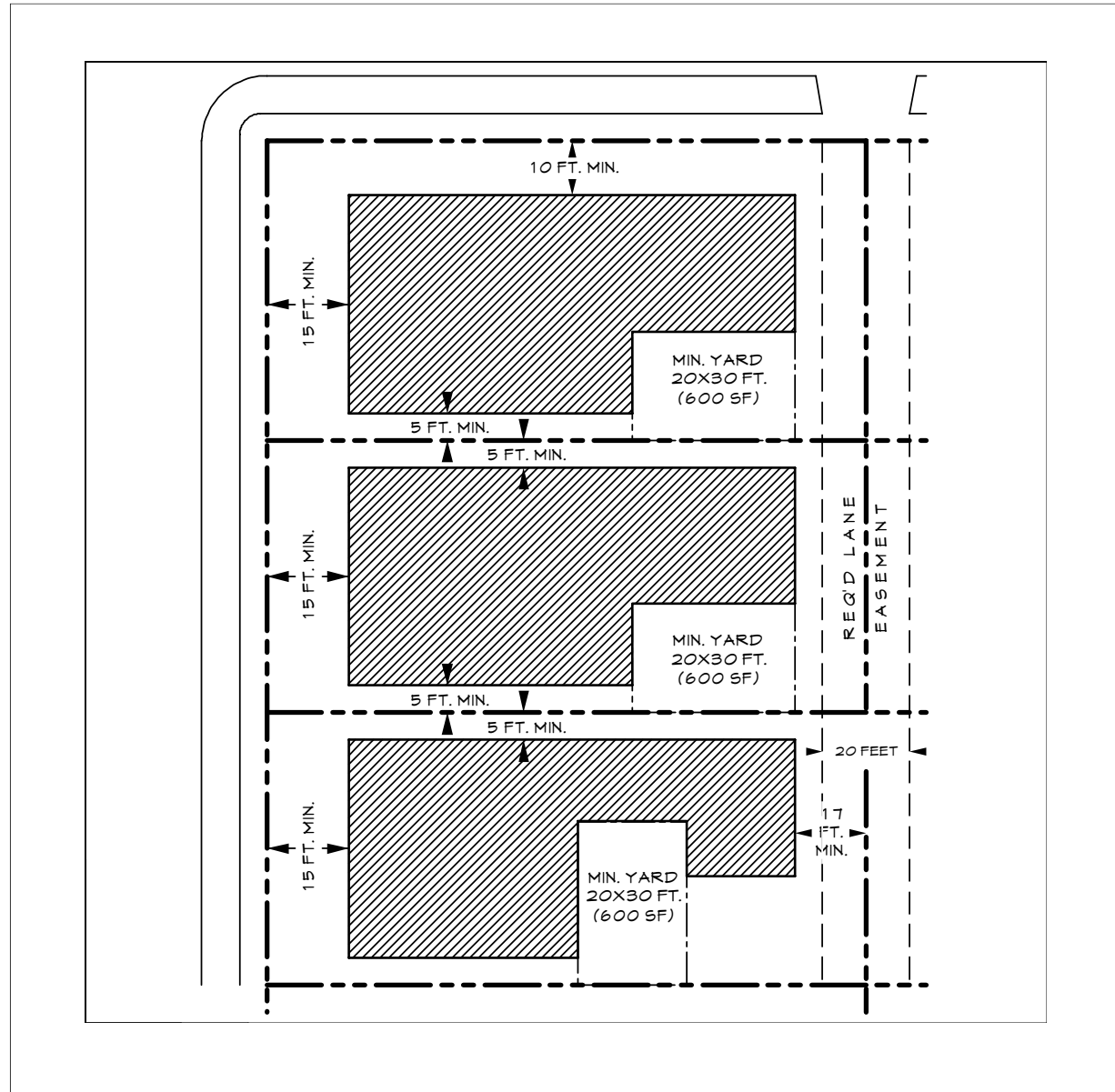
- 1. Maximum building heights shall be measured relative to the sidewalk elevation and a specified point as shown here.
- 2. The height of eaves and ridges for building additions shall substantially match the heights of the original building, as approved by the Director.
- 3. Note that the main floor of the house, and front porch if provided, shall be raised above the sidewalk as shown here.



3.6.3 BUILDING TYPE I-C

3.6.3.1 BUILDING PLACEMENT

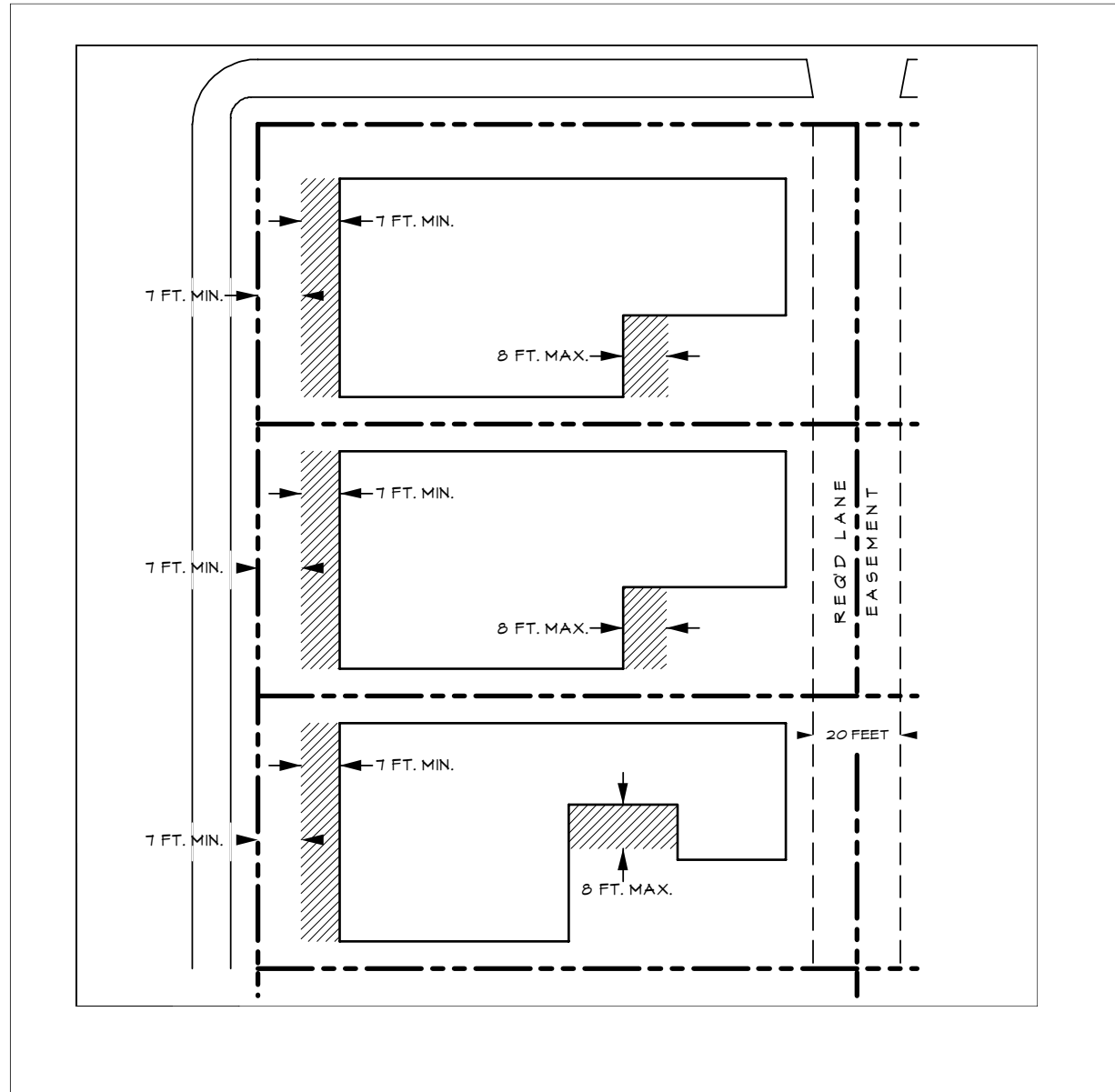
1. Buildings shall be set on lots relative to property lines as shown here.
2. Building Facades shall extend along the Frontages as shown here.
3. Minimum required enclosed yard areas shall be provided as shown here.
4. Required enclosed yards shall be surrounded by fences or garden walls conforming to the requirements of the Architectural Regulations. See Building Elements, below, for additional requirements for wall and fences.
5. This building type may be built only on lots that are provided with alley access. It is intended for lots that are over 50 feet in width, but may be built on narrower lots also.



3.6.3 BUILDING TYPE I-C

3.6.3.2 BUILDING ELEMENTS

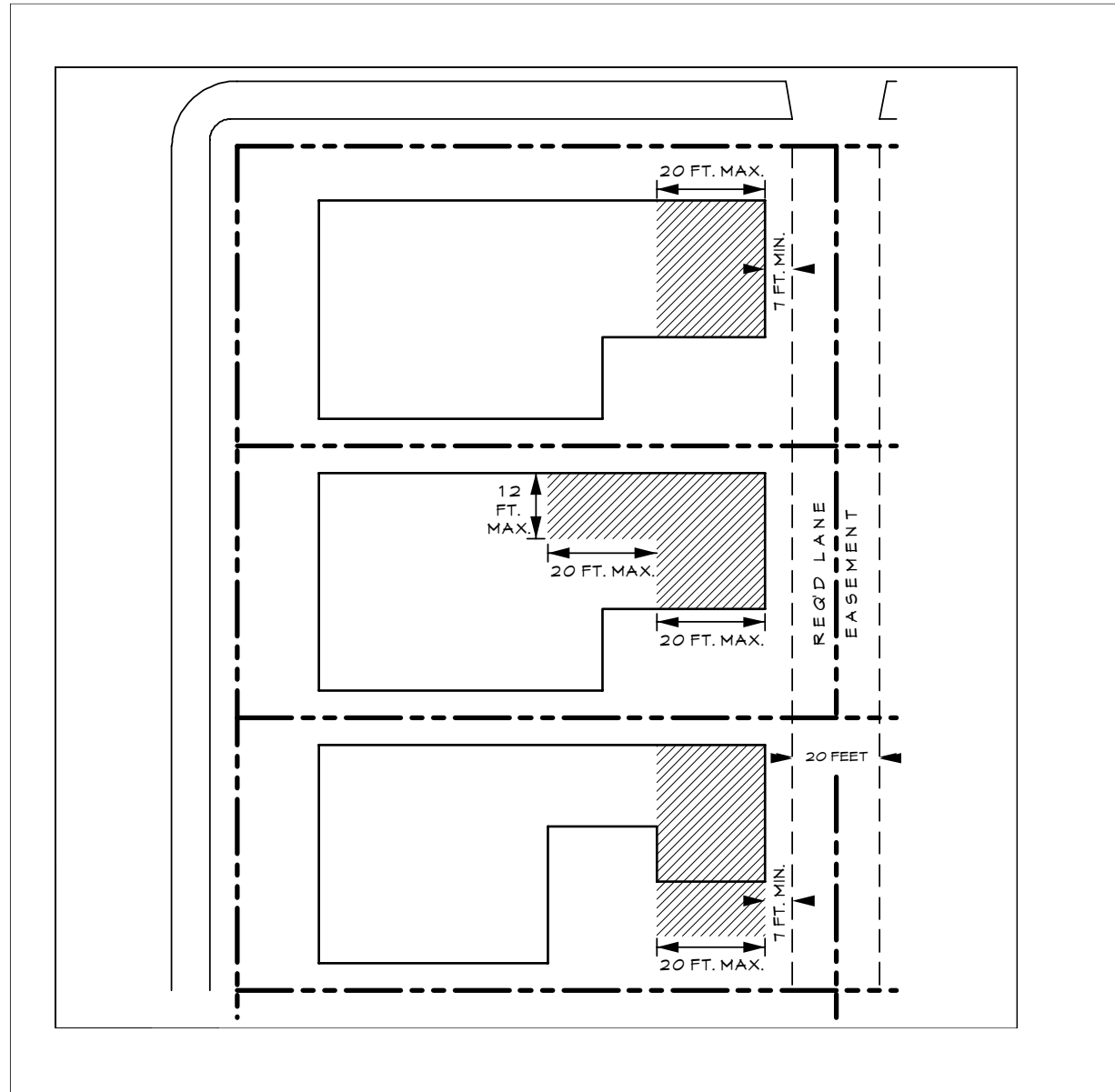
1. Front porches may encroach up to 8 feet into the front yard setback area.
2. Front porches shall be a minimum clear area of 7 feet by 12 feet.
3. Stoops may encroach up to 6 feet into the Front Yard setback area.
4. Pergolas and back porches attached to the primary building may encroach up to 8 feet into the required enclosed yard area. Such encroachments shall not exceed 12 feet in width.
5. Gazebos may be constructed within required enclosed yard areas, provided that they be set back a minimum of 5 feet from an adjoining lot.
6. Walls and fences within 10 feet of a Frontage Line may be between 3 feet and 3 feet 6 inches in height. Walls and fences 10 or more feet from a frontage line may be up to 6 feet in height.
7. Required enclosed yard areas shall be free of parking. As shown in the lower lot on the illustration here, if a third on-site parking space is provided, the required enclosed yard area must move closer to the front of the lot. This reduces the allowable footprint area of the house.



3.6.3 BUILDING TYPE I-C

3.6.3.3 PARKING

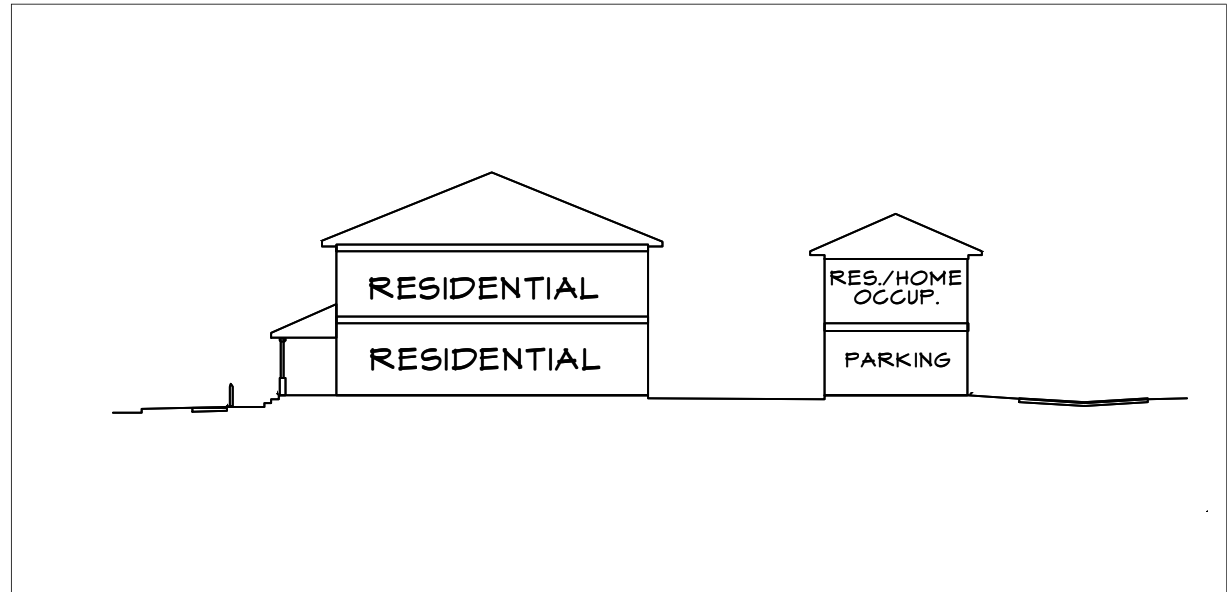
1. Off-street parking spaces shall be placed only within the areas shown here. Parking spaces shall not be less than 10 feet from the side-street frontage lines, not less than 5 feet from interior side property lines. Note that parking is only permitted within the rear 37 feet of each lot.
2. On-street parking spaces along the Frontage shall meet the requirement for guest parking.
3. Off-street parking spaces shall be no less than 9 feet by 19 feet.
4. 2 parking spaces within a garage shall be provided on each lot. Accessory dwelling units (second units) shall be provided with 1 additional on-site parking space, which may be open or covered.
5. Trash containers and enclosures shall be located within the areas where parking is permitted.
6. Required enclosed yard areas shall be free of parking. As shown in the lower lot on the illustration here, if a third on-site parking space is provided, the required enclosed yard area must move closer to the front of the lot. This reduces the allowable footprint area of the house.



3.6.3 BUILDING TYPE I-C

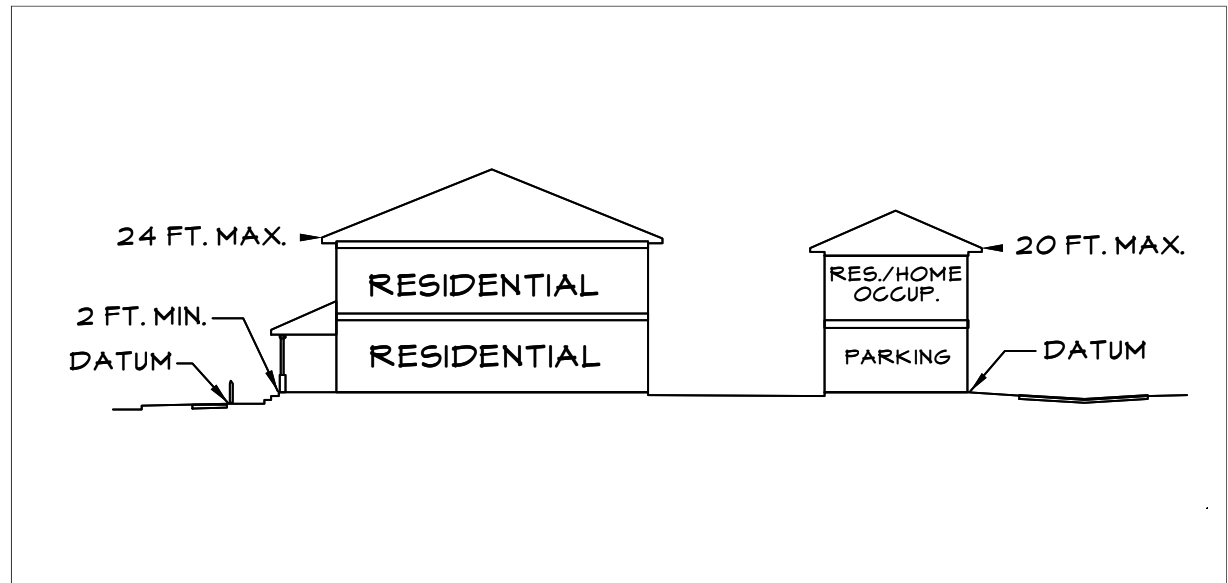
3.6.3.4 BUILDING USE

- 1. Uses permitted within the buildings shall be as shown here.
- 2. Home occupation uses shall be approved by the Director as part of the business license issuance process. See Home Occupation in Definitions.



3.6.3.5 BUILDING HEIGHT

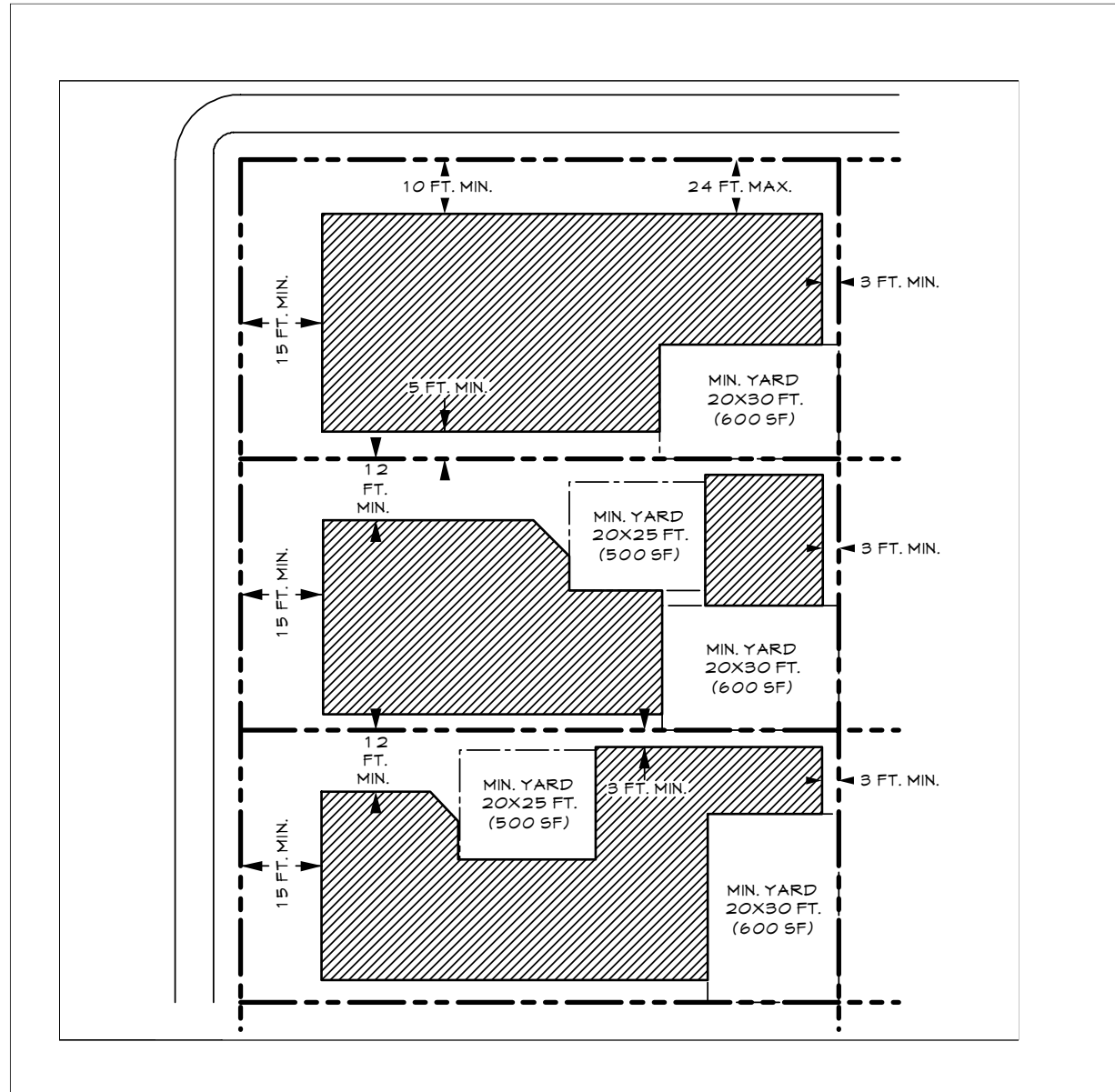
- 1. Maximum building heights shall be measured relative to the sidewalk elevation and a specified point as shown here.
- 2. The height of eaves and ridges for building additions shall substantially match the heights of the original building, as approved by the Director.
- 3. Note that the main floor of the house, and front porch if provided, shall be raised above the sidewalk as shown here.



3.6.4 BUILDING TYPE I-D

3.6.4.1 BUILDING PLACEMENT

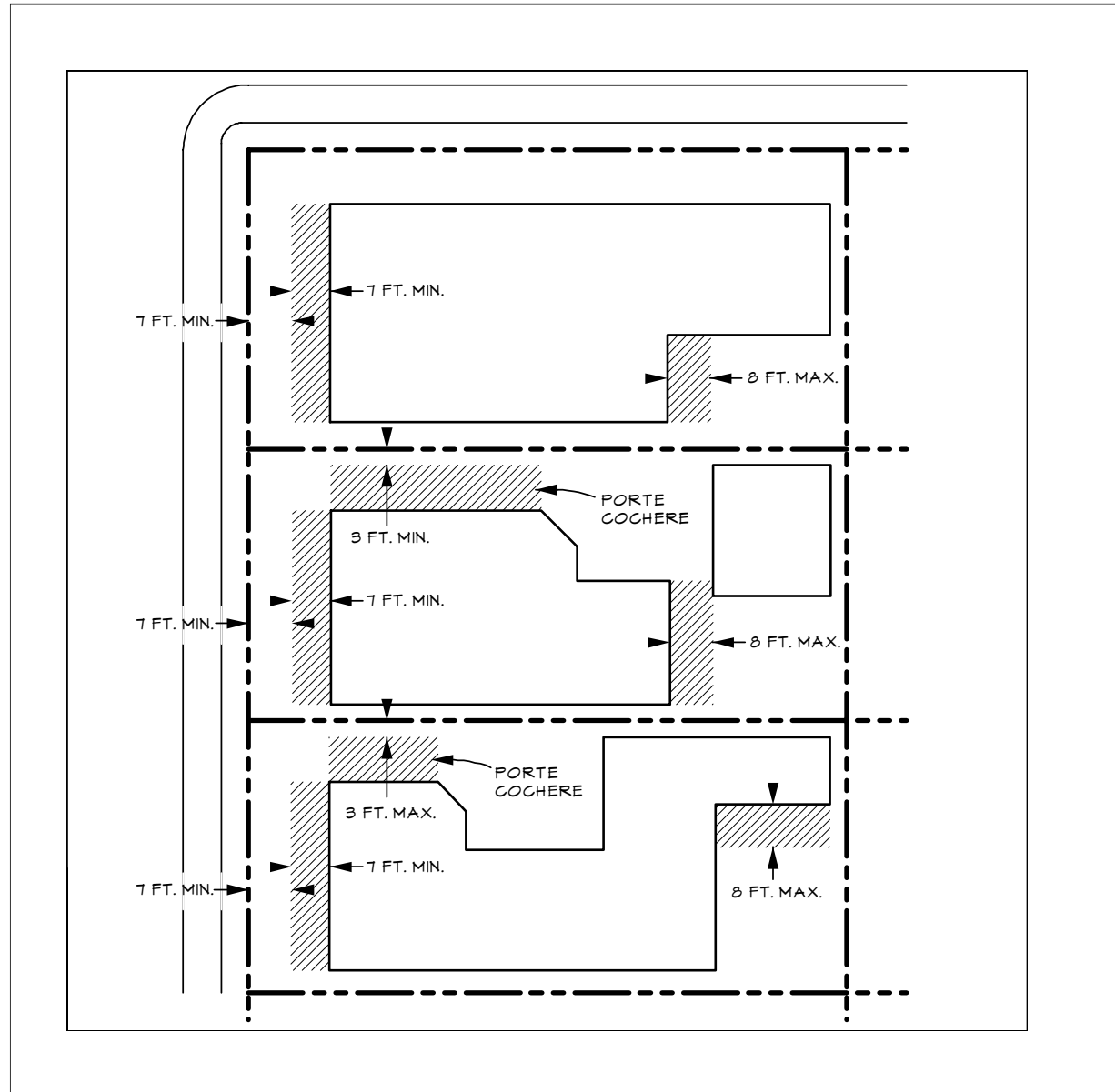
1. Buildings shall be set on lots relative to property lines as shown here.
2. Building Facades shall extend along the Frontages as shown here.
3. Minimum required enclosed yard areas shall be provided as shown here.
4. Required enclosed yards shall be surrounded by fences or garden walls conforming to the requirements of the Architectural Regulations. See Building Elements, below, for additional requirements for wall and fences.
5. The 3-foot minimum side and rear yard setbacks shown here apply to 1-story building elements. The minimum requirement for 2-story elements is 5 feet.



3.6.4 BUILDING TYPE I-D

3.6.4.2 BUILDING ELEMENTS

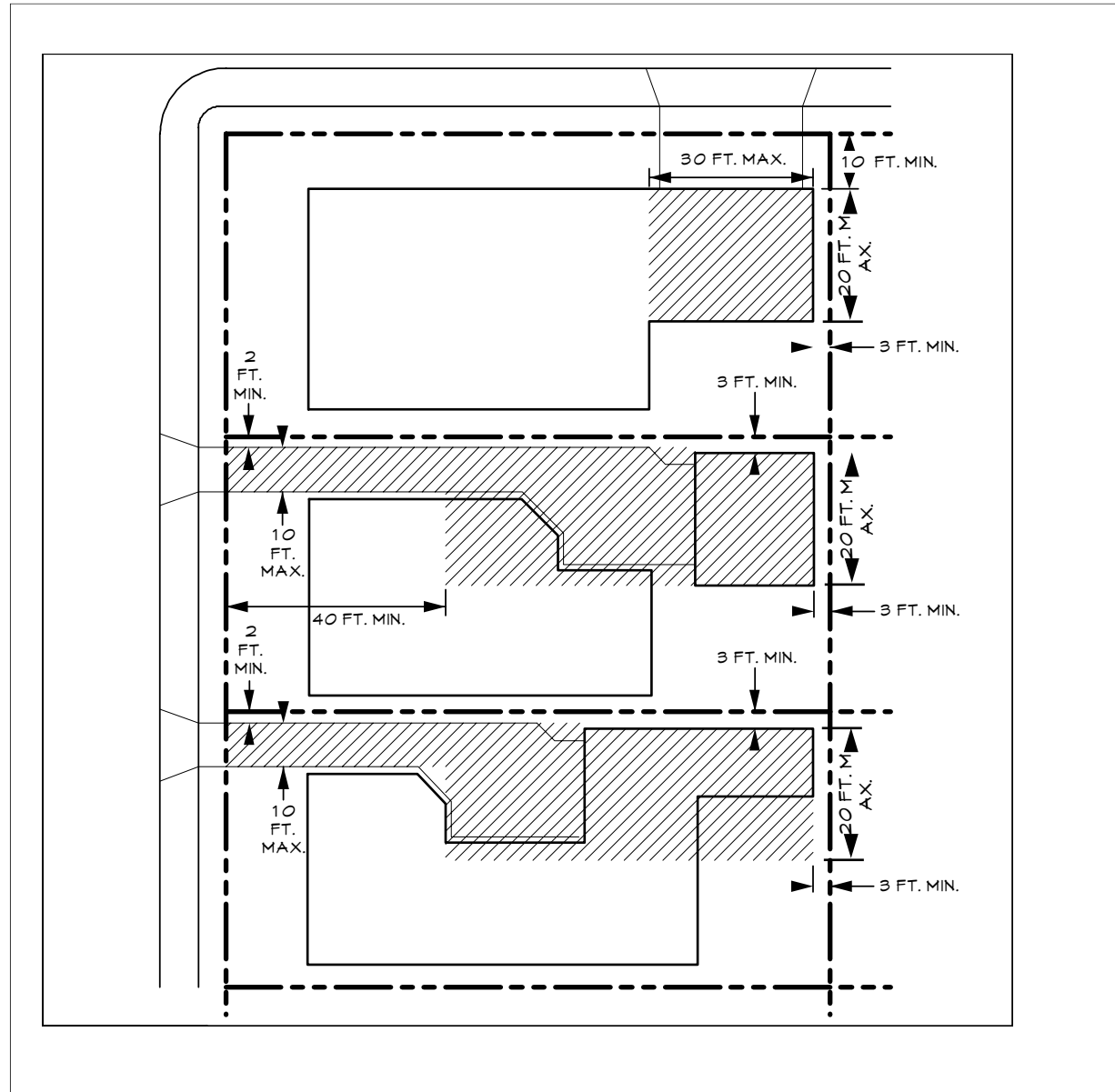
1. Front porches may encroach up to 8 feet into the front yard setback area and shall be a minimum clear area of 7 feet by 12 feet.
2. Portes cocheres may be built over driveways, to within 2 feet of the side property line.
3. Pergolas and back porches attached to the primary building may encroach up to 8 feet into the required enclosed yard area. Such encroachments shall not exceed 12 feet in width.
4. Gazebos may be constructed within required enclosed yard areas, provided that they be set back a minimum of 5 feet from an adjoining lot.
5. Chimneys may encroach up to 2 feet into Side Yard and Rear Yard setback areas. Roof overhangs may encroach up to 2 feet into any required setback area.
6. Walls and fences within 10 feet of a Frontage Line may be between 3 feet and 3 feet 6 inches in height. Walls and fences 10 or more feet from a frontage line may be up to 6 feet in height.
7. Freestanding porte-cocheres, detached from the house, may be approved through the Design Review Process on lots with sufficient width to accommodate them.



3.6.4 BUILDING TYPE I-D

3.6.4.3 PARKING

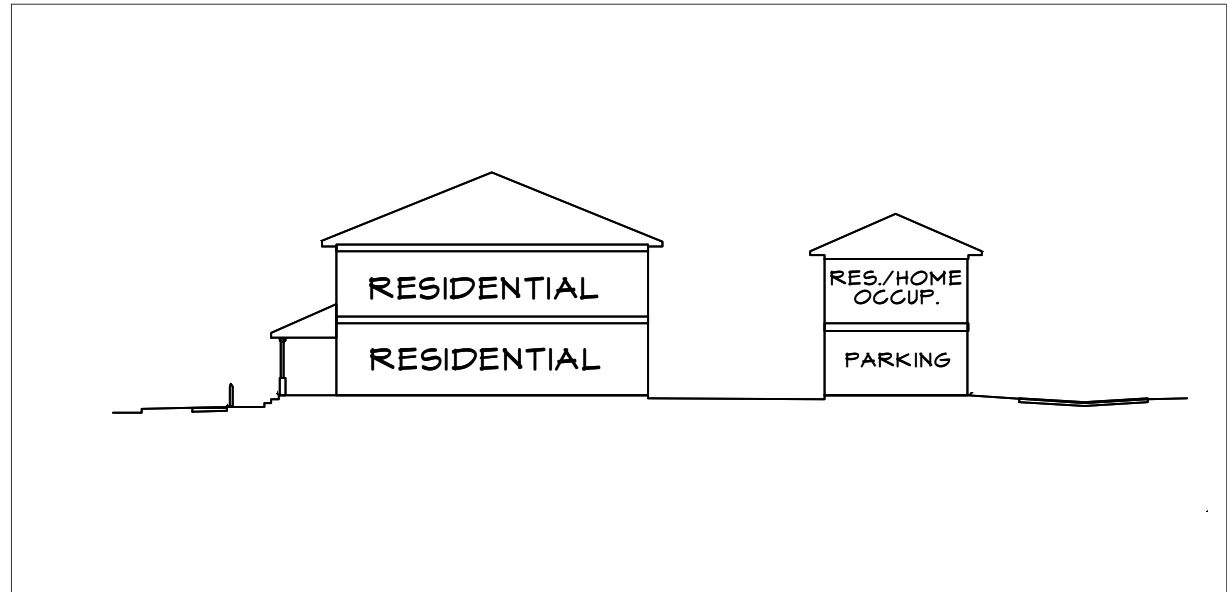
1. Off-street parking spaces shall be placed only within the areas shown here.
2. On-street parking spaces along the Frontage shall meet the requirement for guest parking.
3. Off-street parking spaces shall be no less than 9 feet by 19 feet.
4. 2 parking spaces within a garage shall be provided on each lot. Accessory dwelling units (second units) shall be provided with 1 additional on-site parking space, which may be open or covered.
5. Trash containers and enclosures shall be located within the areas where parking is permitted.
6. Required enclosed yard areas shall be free of parking. As shown in the lower lot on the illustration here, if a third garage parking space is provided in tandem, the primary two-car garage must move closer to the front of the lot. This reduces the allowable footprint area of the house.
7. Note that the garage and the back-out area of the driveway must be at least 40 feet from the frontage line. The driveway pavement within 40 feet of the frontage may not exceed 10 feet in width – 8 feet is the recommended width. Drives may have a grass strip in the middle.



3.6.4 BUILDING TYPE I-D

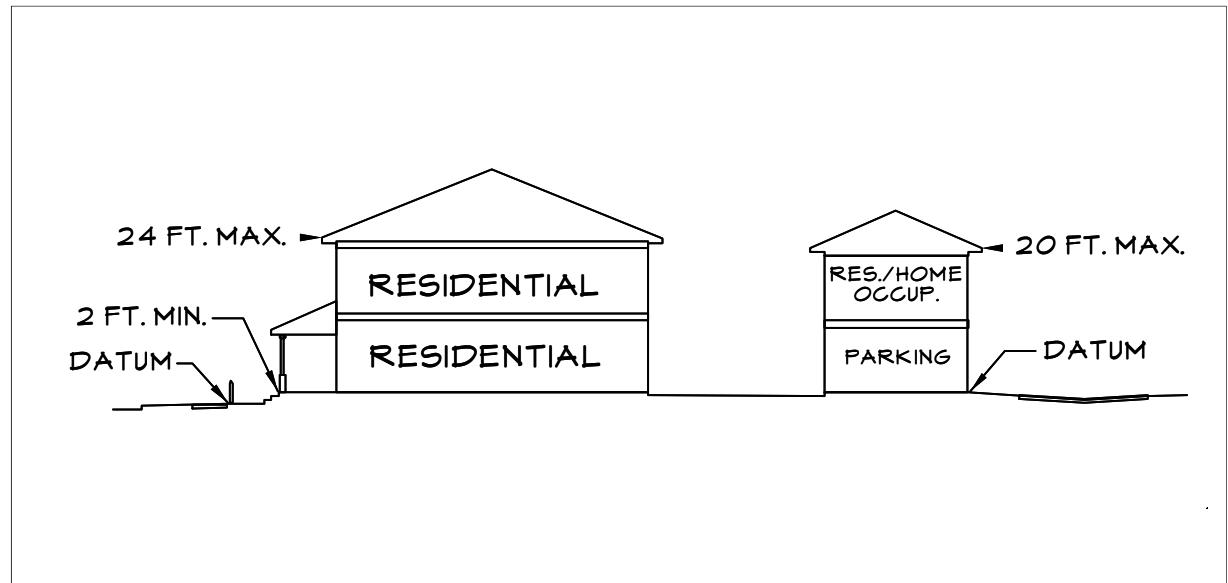
3.6.4.4 BUILDING USE

- 1. Uses permitted within the buildings shall be as shown here.
- 2. Home occupation uses shall be approved by the Director as part of the business license issuance process. See Home Occupation in Definitions.



3.6.4.5 BUILDING HEIGHT

- 1. Maximum building heights shall be measured relative to the sidewalk elevation and a specified point as shown here. The datum point is taken at the highest elevation of the frontage line on sloping lots. See Architectural Regulations for additional requirements.
- 2. The height of eaves and ridges for building additions shall substantially match the heights of the original building, as approved by the Director.
- 3. Note that the main floor of the house, and front porch if provided, shall be raised above the sidewalk as shown here.

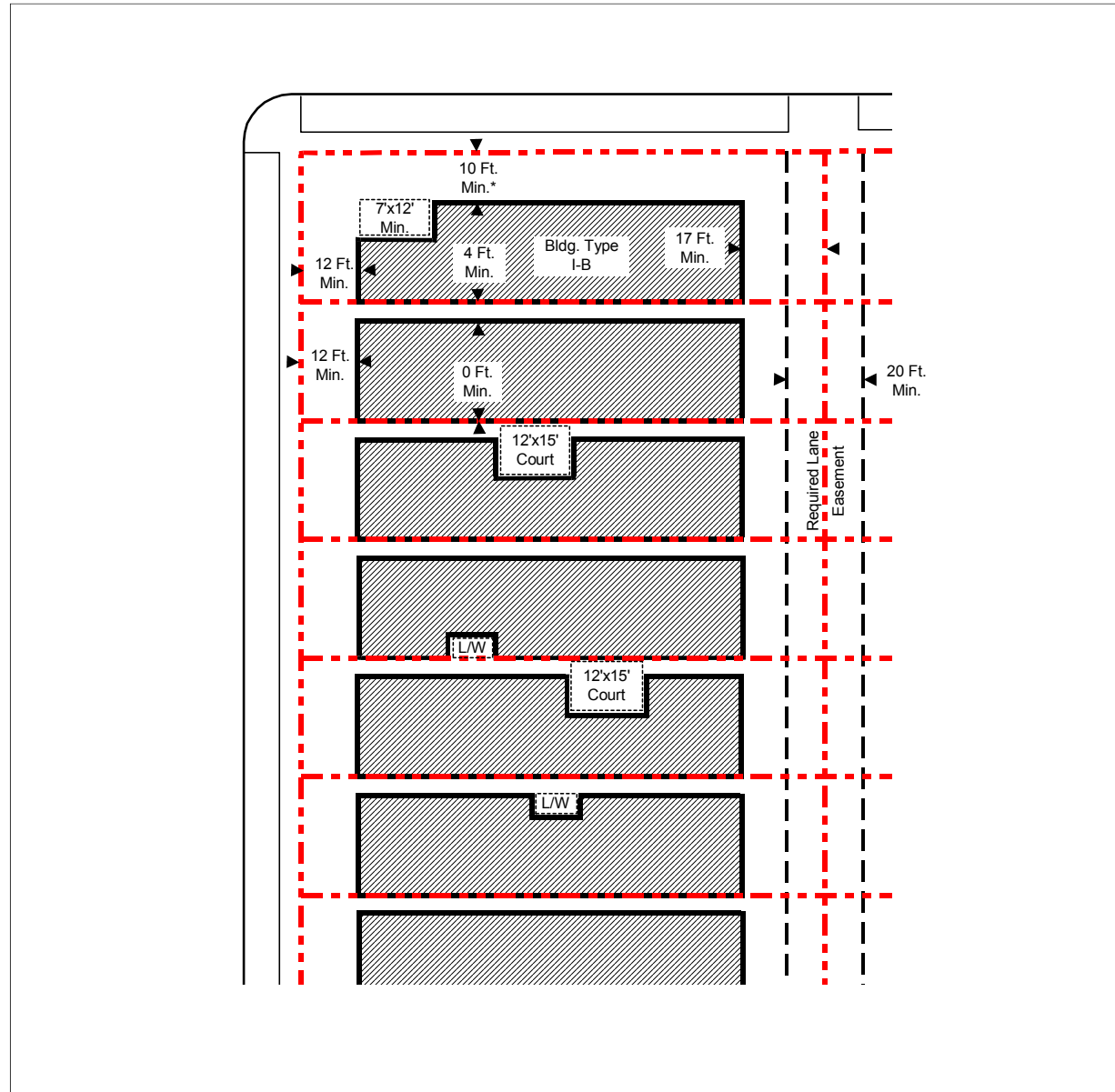


3.6.5 BUILDING TYPE I-E

3.6.5.1 BUILDING PLACEMENT

1. Buildings shall be set on lots relative to property lines as shown by hatched areas in the diagram here.
2. This house type is intended for lots between 24 and 28 feet in width.
3. Building Type I-B is required at corner lots - see section 3.6.2. If the lot is wide enough to accommodate a third parking space at the rear (see section 3.6.2.3) then a second dwelling unit may be provided over the garage as noted in section 3.6.2.4.
4. Private courtyards are not required, but if provided the sizes shown here are the recommended minimum.
5. In addition to courts, light wells may be provided in either side of the house, as permitted by the Building Code. The recommended minimum size is 3 by 5 feet.
6. Note that no sideyard setback is required on one side of the lot, which shall be the downhill side. The grade difference between the lots shall be accommodated by the footing of the uphill house rather than by a separate retaining wall, so that the elevation of the sideyard is at or below the elevation of the house on that lot.

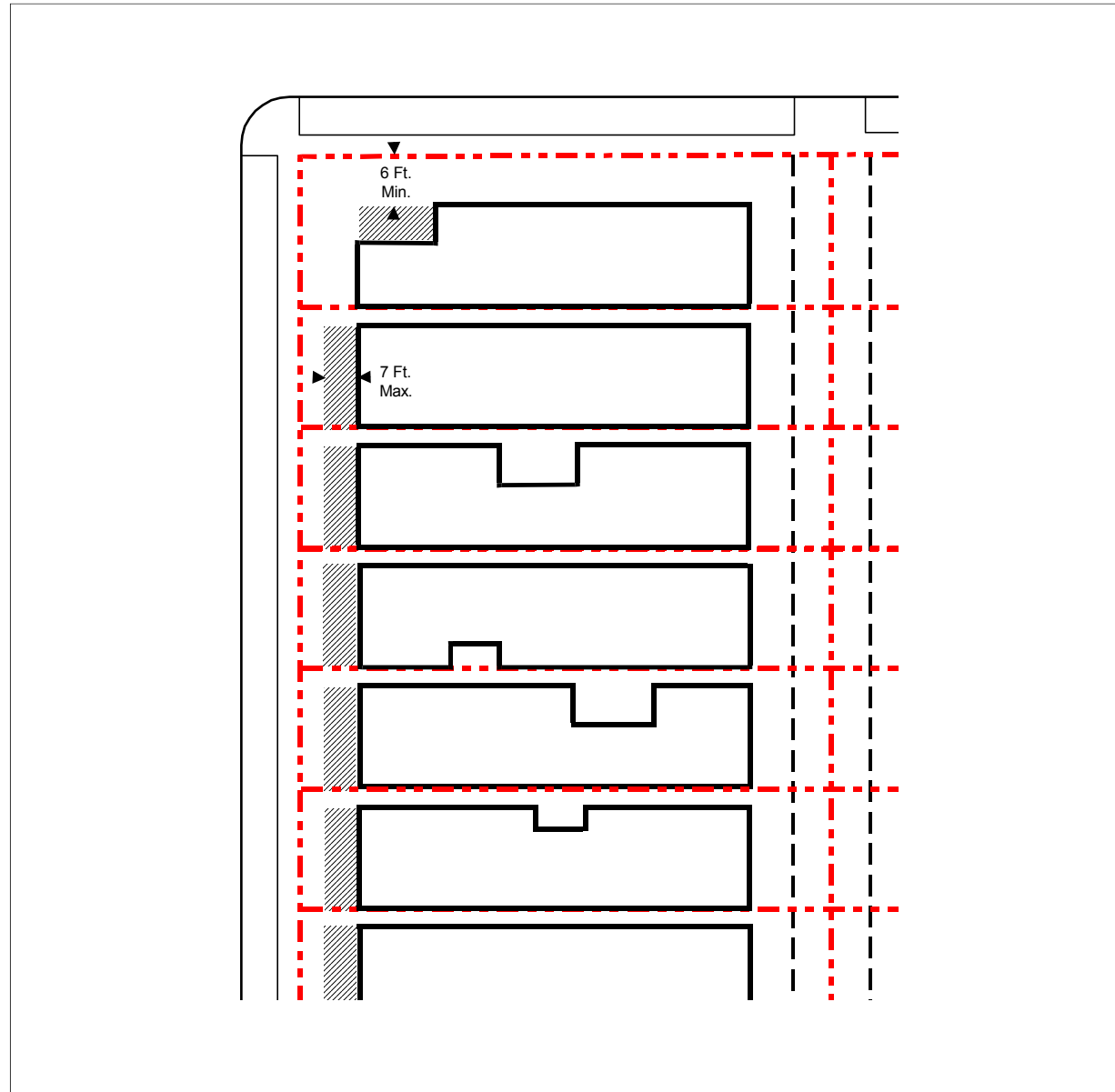
* Building Type I-B is required at lots adjacent to mid-block lanes and pedestrian paseos, with the porch facing the lane or paseo. The minimum setback to the lane or paseo is 6 feet.



3.6.5 BUILDING TYPE I-E

3.6.5.2 BUILDING ELEMENTS

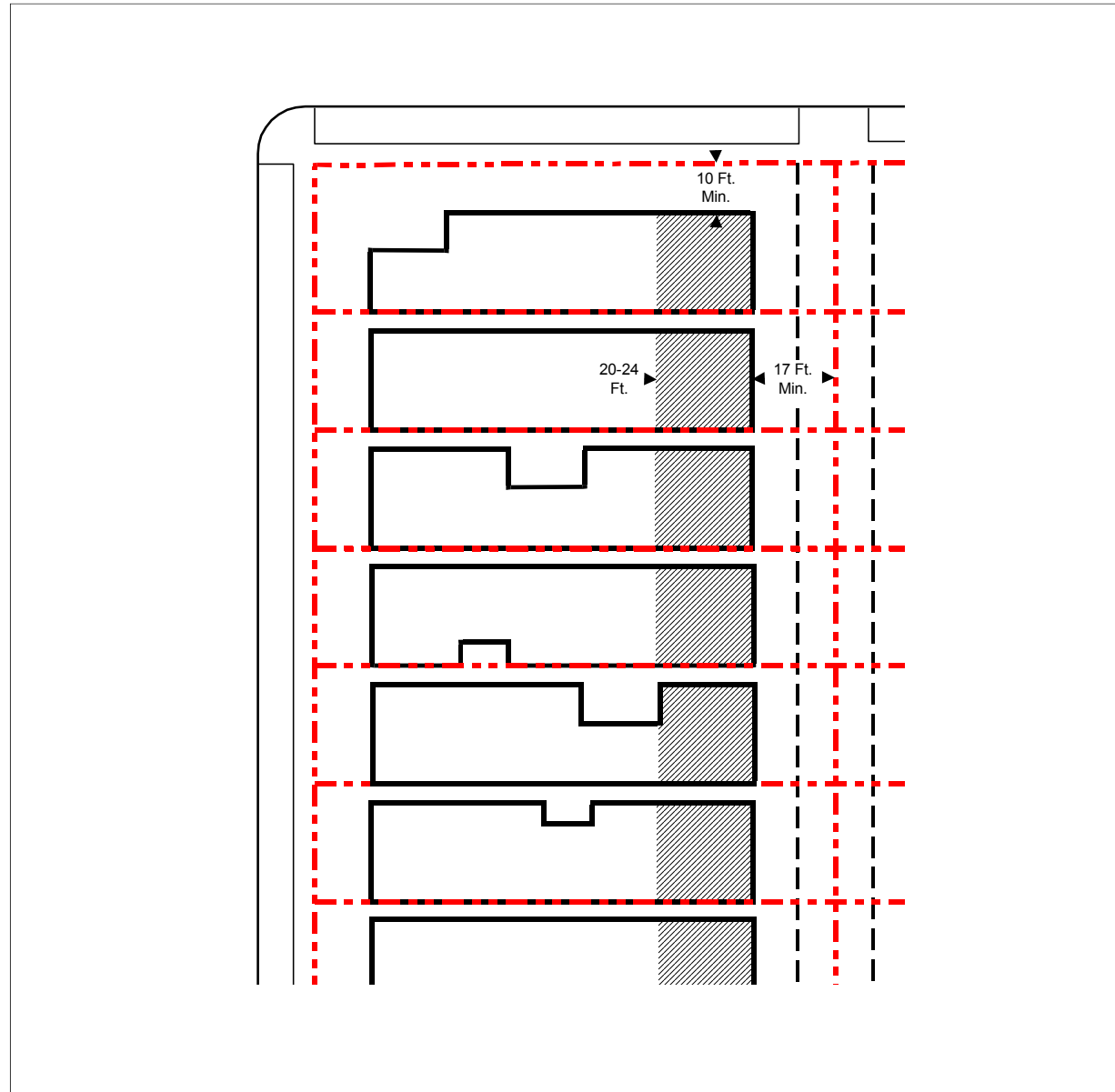
1. Front porches may encroach up to 7 feet into the front yard setback area and, when provided, shall have a minimum clear area of 6 feet by 8 feet. Porches at corner lots may encroach up to 4 feet into the required side yard setback.
2. Roof overhangs may encroach into required setback areas as shown in the Architectural Regulations. Note that due to the narrowness of the lots and the absence of sideyard setbacks in some cases, the Uniform Building Code (U.B.C.) may limit or prohibit roof overhangs at side property lines. In such cases, it is recommended that principal roofs slope to the front and rear of the lot, so that eaves conforming with the requirements of the Architectural Regulations can be provided at the front and rear elevations. The overhangs at the rake may be limited to a trim board, or as approved through the Design Review Process.
3. Walls and fences within the front yard setback area may be between 3 feet and 3 feet 6 inches in height. Sideyard gates between the houses may be up to 6 feet in height, and shall be designed as integral elements of the building architecture.



3.6.5 BUILDING TYPE I-E

3.6.5.3 PARKING

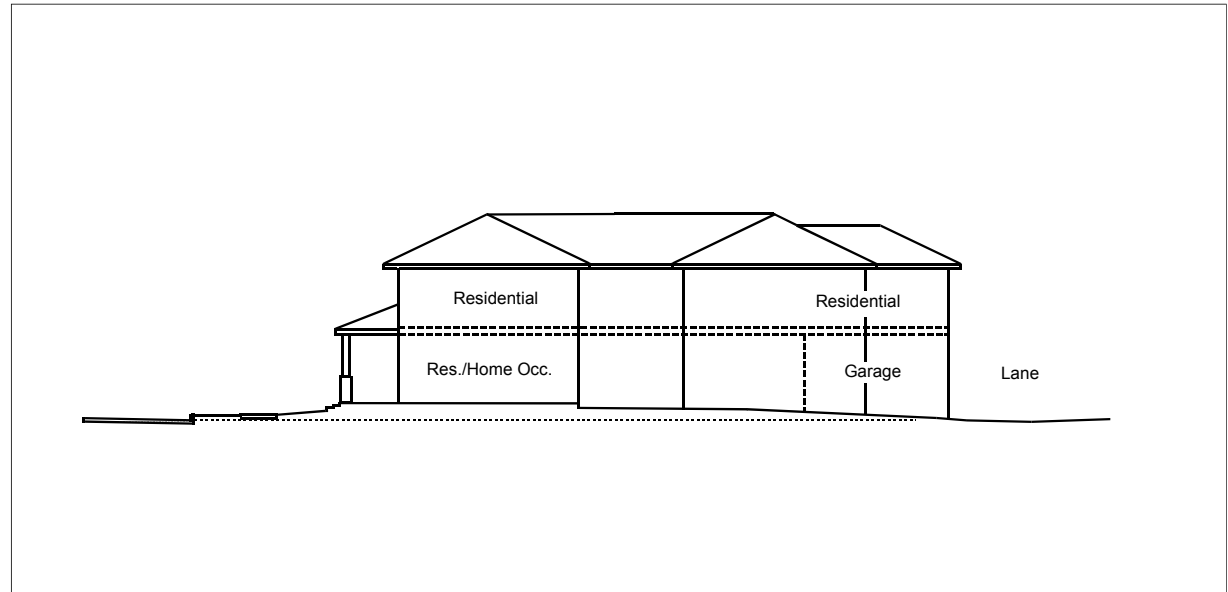
1. Off-street parking spaces shall be placed only within the hatched areas shown here.
2. On-street parking spaces along the Frontage shall meet the requirement for guest parking.
3. Off-street parking spaces shall be no less than 9 feet by 19 feet.
4. Two parking spaces within a garage shall be provided on each lot. A third space, which may be enclosed or open, shall be provided at houses on corner lots if a second dwelling unit is provided over the garage. See Building Type I-B, section 3.6.2.
5. Trash containers and enclosures shall be located at the rear of the lot.



3.6.5 BUILDING TYPE I-E

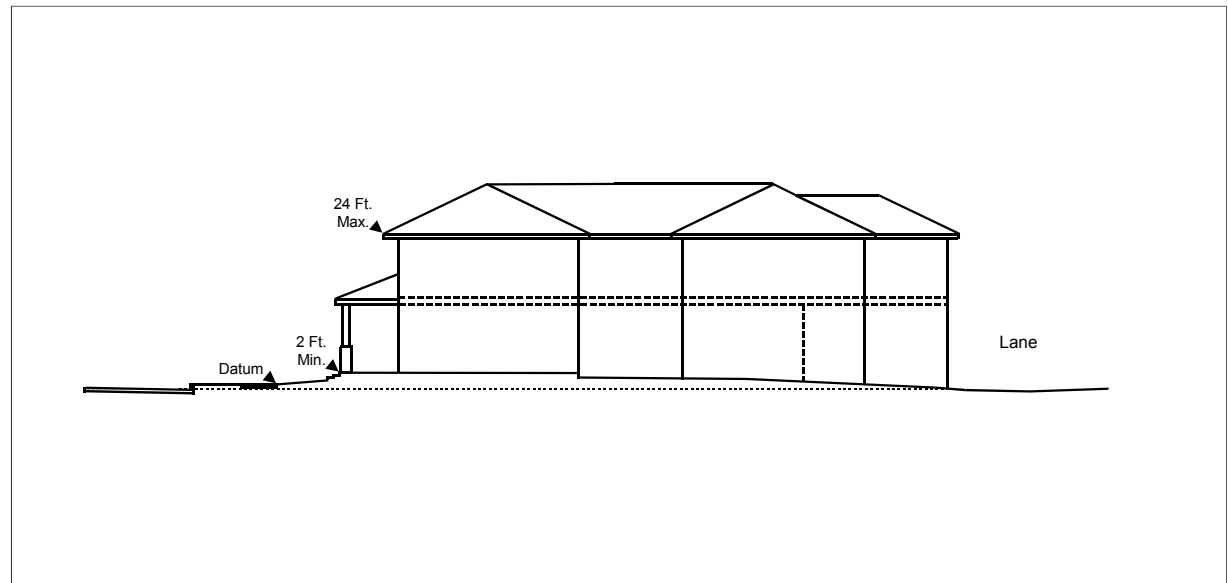
3.6.5.4 BUILDING USE

1. Uses permitted within the buildings shall be as shown here.
2. Home occupation uses shall be approved by the Director as part of the business license issuance process. See Home Occupation in Definitions.
3. A second independent dwelling is permitted over the garage at corner lots if at least three on-site parking spaces are provided (see Building Type I-B).



3.6.5.5 BUILDING HEIGHT

1. Maximum building heights shall be measured relative to the sidewalk elevation and a specified point as shown here. The datum point is taken at the highest elevation of the frontage line on sloping lots. See Architectural Regulations for additional requirements.
2. The height of eaves and ridges for building additions shall substantially match the heights of the original building, as approved by the Director.
3. Note that the main floor of the house, and front porch if provided, shall be raised above the sidewalk as shown here.

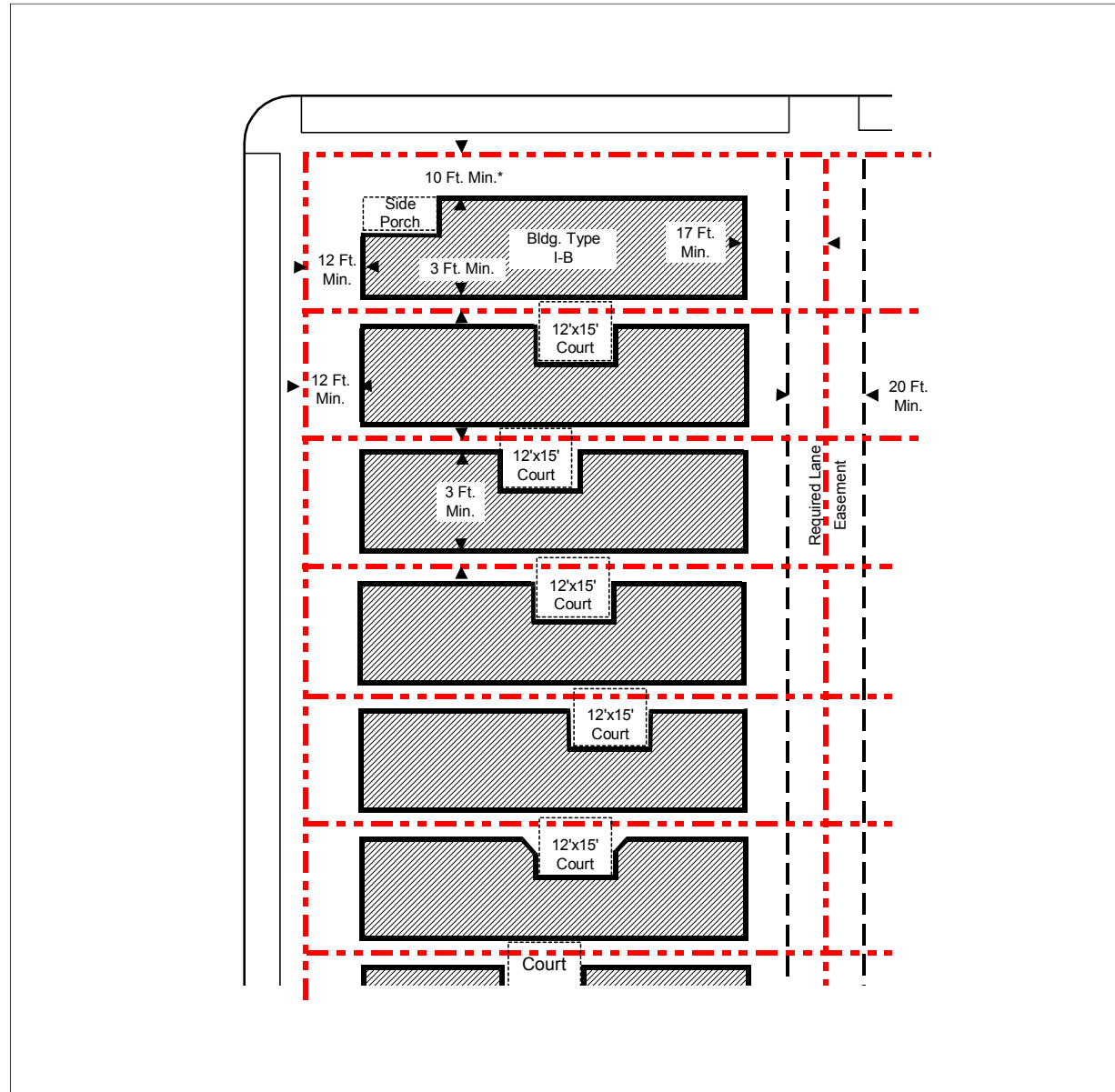


3.6.5 BUILDING TYPE I-F

3.6.5.6 BUILDING PLACEMENT

1. Buildings shall be set on lots relative to property lines as shown by hatched areas in the diagram here.
2. This house type is intended for lots at least 27 feet in width.
3. Building Type I-B is required at corner lots - see section 3.6.2. If the lot is wide enough to accommodate a third parking space at the rear (see section 3.6.2.3) then a second dwelling unit may be provided over the garage as noted in section 3.6.2.4.
4. Private courtyards are recommended as shown here - the recommended minimum sizes are as noted. Courts shall be at or below the ground floor elevation of the house, and may extend to the wall of the adjacent house if appropriate irrevocable yard agreements are recorded against both lots. Wood decks - at or below the ground floor level - and hardscape are permitted in courts in addition to landscaping. Courts shall not be roofed.
5. The grade difference between the lots shall be accommodated by the footing of the uphill house rather than by a separate retaining wall.

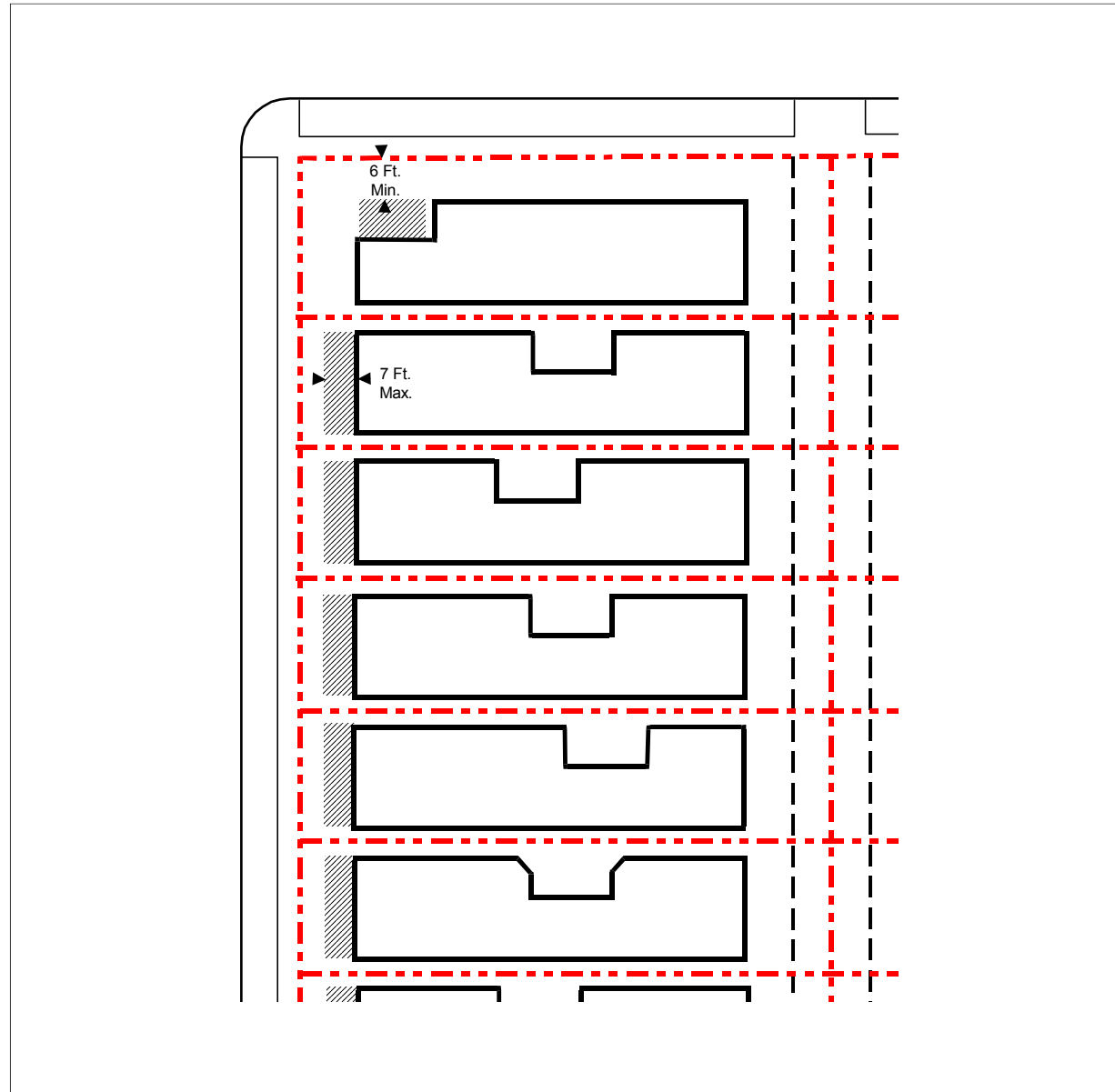
* Building Type I-B is required at lots adjacent to midblock lanes and pedestrian paseos, with the porch facing the lane or paseo. The minimum setback to the lane or paseo is 6 feet.



3.6.5 BUILDING TYPE I-F

3.6.5.7 BUILDING ELEMENTS

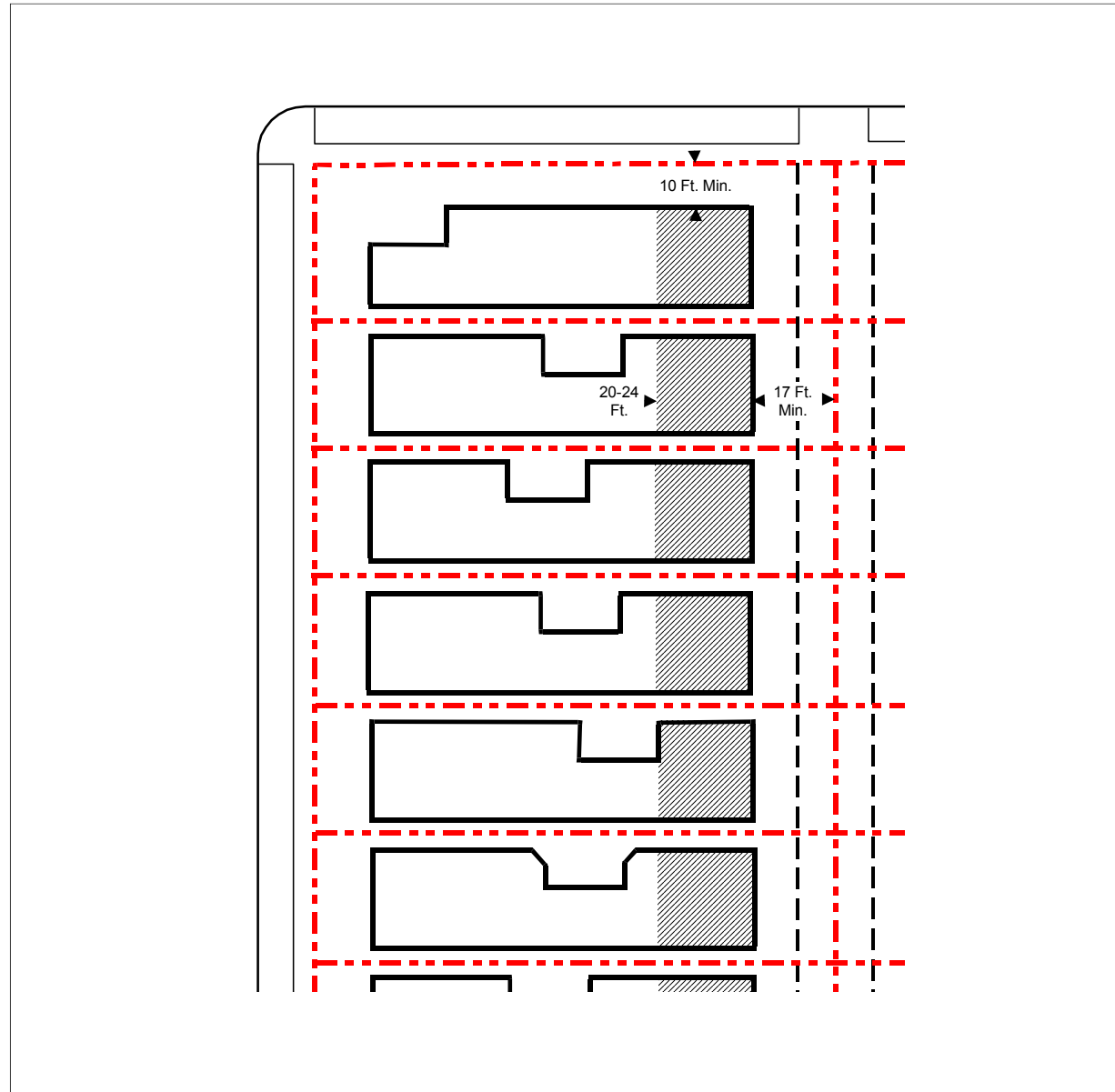
1. Front porches may encroach up to 7 feet into the front yard setback area and, when provided, shall be a minimum clear area of 6 feet by 8 feet. Porches at corner lots may encroach up to 4 feet into the required side yard setback.
2. Roof overhangs and cornices may encroach into required setback areas as shown in the Architectural Regulations. See the Architectural Regulations for the required dimensions and configurations of overhangs.
3. Walls and fences within the front yard setback area may be between 3 feet and 3 feet 6 inches in height. Sideyard gates between the houses may be up to 6 feet in height, and shall be designed as an integral element of the building architecture. Side yard fences are discouraged; see discussion of courtyards under Building Placement.



3.6.5 BUILDING TYPE I-F

3.6.6.8 PARKING

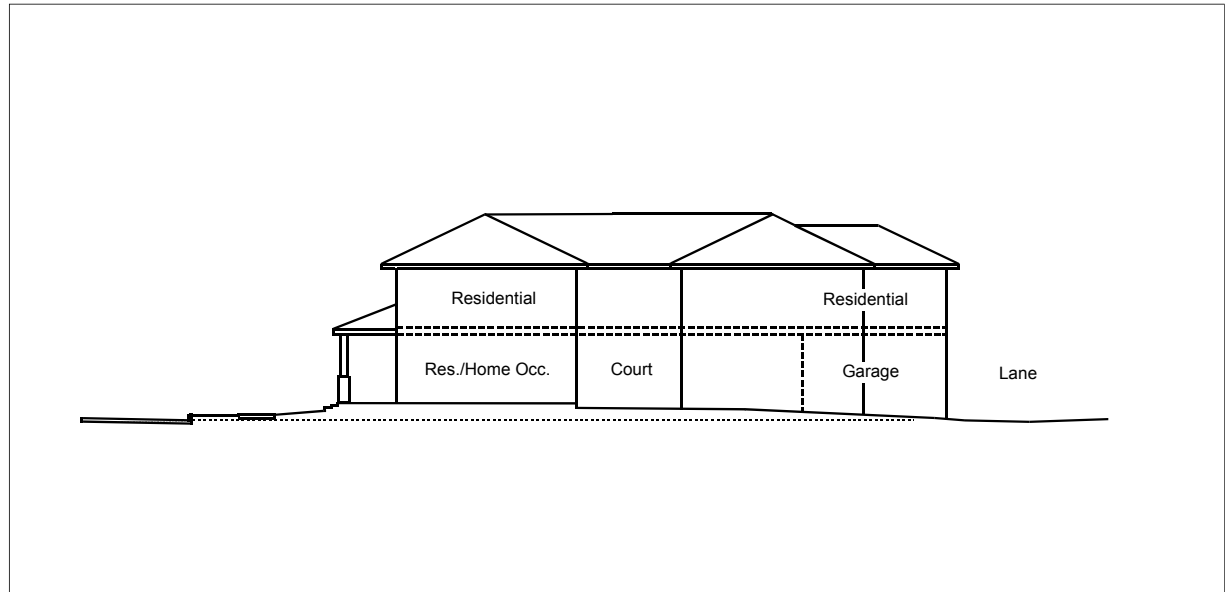
1. Off-street parking spaces shall be placed only within the hatched areas shown here.
2. On-street parking spaces along the Frontage shall meet the requirement for guest parking.
3. Off-street parking spaces shall be no less than 9 feet by 19 feet.
4. Two parking spaces within a garage shall be provided on each lot. A third space, which may be enclosed or open, shall be provided at houses on corner lots if a second dwelling unit is provided over the garage. See Building Type I-B, section 3.6.2.
5. Trash containers and enclosures shall be located at the rear of the lot.



3.6.5 BUILDING TYPE I-F

3.6.5.9 BUILDING USE

1. Uses permitted within the buildings shall be as shown here.
2. Home occupation uses shall be approved by the Director as part of the business license issuance process. See Home Occupation in Definitions.
3. A second independent dwelling is permitted over the garage at corner lots if at least three on-site parking spaces are provided (see Building Type I-B).



3.6.5.10 BUILDING HEIGHT

1. Maximum building heights shall be measured relative to the sidewalk elevation and a specified point as shown here. The datum point is taken at the highest elevation of the frontage line on sloping lots. See Architectural Regulations for additional requirements.
2. The height of eaves and ridges for building additions shall substantially match the heights of the original building, as approved by the Director.
3. Note that the main floor of the house, and front porch if provided, shall be raised above the sidewalk as shown here.

